



# CITY OF CHARLESTON TRC COMMENTS/RESULTS

Meeting Date: 5/18/2023

Agenda Item # 1

UNION PIER

PUD MASTER PLAN

Concord St.

459-00-00-009

2nd Review

TRC Project ID#

PUD2023-000025

To:

Randolph Lowell  
South Carolina Ports Authority  
rlowell@burr.com

From:

Eric Schultz, TRC Administrator  
Dept. of Planning, Preservation, and Sustainability  
schultze@charleston-sc.gov  
843-724-3790

	Comments	Paperwork and/or Permit(s)	No Comments	Staff Initials
Zoning	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>JCS.</i>
Parks	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<i>R.P.</i>
ADA	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<i>JCS.</i>
Traffic and Transportation	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>JCS.</i>
Engineering	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<i>JCS.</i>
Stormwater	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<i>JCS.</i>
GIS/Addressing	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<i>JCS.</i>
Planning	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>JCS.</i>
Fire Marshal	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<i>JCS.</i>

APPLICANT ATTENDED MEETING

WRITTEN COMMENTS PROVIDED

REVISE PLANS AND RESUBMIT TO TRC. SUBMIT .PDF FILES OF REVISED PLANS, RESPONSES TO COMMENTS, COMPLETED APPLICATION, AND OTHER DOCUMENTATION AS REQUIRED. APPLICATION FEE REQUIRED.

REVISE AND SEND .PDF SUBMITTAL TO TRC COORDINATOR. ONCE COMMENTS RESOLVED, SUBMIT .PDF FOR STAMPING:

PLAT: PLANNING

SITE PLAN: ZONING

ROAD PLANS / R/W CONSTRUCTION: ENGINEERING

APPROVED. SUBMIT FINAL .PDF FOR STAMPING:

PLAT: PLANNING

SITE PLAN: ZONING

ROAD PLANS / R/W CONSTRUCTION: ENGINEERING

*REVISE / RETURN*

# City of Charleston

Americans with Disabilities Act (ADA) Coordinator  
Comments for Technical Review Committee

Union Pier PUD  
City Project ID # PUD2023-000025

1. With a goal of comprehensive simplicity, I request that the ADA related statements in the PUD be struck and in Section 4.1 please include:

The development of the Union Pier Site and properties subject to the Union Pier PUD will meet all ADA guidelines including but not limited to the ADAAS (ADA Accessibility Standards), FHA, building code, and City of Charleston codes.

Janet Schumacher, ADA Coordinator  
50 Broad Street Charleston, SC 29401 (843) 577-1389  
[schumacherj@charleston-sc.gov](mailto:schumacherj@charleston-sc.gov)



**Date:** 02-Mar-23, **18-May-23**  
**To:** South Carolina Ports Authority  
**From:** Moshtaba Vedad, Civil Engineer I  
[vedadm@charleston-sc.gov](mailto:vedadm@charleston-sc.gov) or 843-619-6086

**Project Name:** Union Pier  
**Project Type:** Planned unit Development  
**Project TMS #:** C590-00-00-009  
**Project ID #:** PUD2023-000025  
**Submittal Review #:** 1<sup>st</sup> review – Comments Provided  
2<sup>nd</sup> Review-No Comment

**Documents Reviewed:**

Ref#	Review #	Document Title:	Rev	Ref#	Review #	Document Title:	Rev
SWDSM	all	City of Charleston Storm water Design Standards Manual, January 2020					
A	1 <sup>st</sup> Review 02-Mar-23	PUD2023-000025_1 <sup>st</sup> Review Union Pier PUD.pdf			1 <sup>st</sup> Review 02-Mar-23		
B	2 <sup>nd</sup> Review 18-May-23	PUD2023-000025_2 <sup>nd</sup> Review Union Pier PUD.pdf			2 <sup>nd</sup> Review 18-May-23		

Please provide a response to each review comment below (*match text coloring to review for clarity*) to show how the comment was addressed within your submittal. This will help provide a thorough project review and ensure proper communication between the reviewer and the applicant. Without responses to comments, subsequent submittals may not be reviewed. Resolution column is for City use only – A blank cell in this column indicates issue is still outstanding.

**Please return completed Word document to Engineering Division concurrently with your next TRC (re)submittal.**

#	Sheet/ Page #	Review Comment	Response (Applicant)	Resolution (City Staff)
1	All PDF Files	Please provide functional PDF bookmarks with sheet/page numbers (and abbreviated sheet/chapter titles) to expedite review. Check function of 'action' prior to submitting.	<i>Provided PDF is annotated with correct Bookmarks.</i>	Resolved
2	Cover	Include City PUD ID# (PUD2023-000025) and relevant tax TMS numbers (590-00-00-009) on cover.	<i>Cover updated</i>	Resolved



#	Sheet/ Page #	Review Comment	Response (Applicant)	Resolution (City Staff)
3	Site Plan - Lots	Although individual lots do not have to be shown, the types of lots/uses proposed should be provided (e.g. single-family, townhouse, multi-family, commercial, civic)	<i>This information is provided in the PUD text in Sec. 4.1</i>	Resolved
4	Site Plan	Label landscaped island and any other areas that will be maintained by H.O.A. This needs to be reflected on the Preliminary Plat as well Open space, public use areas, amenity areas and recreational areas, identify location.	<i>This documentation will be provided at the Site Plan level of review.</i>	Resolved
5	Site Plan - Streets	Demonstrate that the topographic survey and boundary survey is properly certified by a Registered Land Surveyor responsible for the preparation of the survey. Show proposed street layout within the development including connections to existing streets and adjacent properties. Streets should be identified as public or private. Note R/W width and provide all right-of-way elements. For PUD projects, label Street Types and note linear footage.	<i>See attached survey, for proposed street layout. see Street Types Plan Ex. A.10, linear footage of all streets to be provided at future Concept Plan application.</i>	Resolved
6	Site Plan - Streets	Rights-of-way/Streets: show the locations, names, and ROW widths of all existing streets within 150 feet of the property boundary with ownership and maintenance (state, county, city, community, private etc.) noted. Show all proposed improvements to existing public roadways (i.e., widening existing streets for turn lanes, etc.). Provide ROW Cross-Section show future roadway length and width, sidewalk width, creating Open Spaces and any parking (width and length).	<i>See attached survey; Proposed street improvements provided in exhibits and Traffic Study; more detailed engineering to be provided in future Concept Plan application.</i>	Resolved
7	Site Plan	Pedestrian trails and bike paths: Show circulation within the Development if it will be in proposed design include connections to existing streets and adjacent properties. Identify whether items will be within or outside of proposed ROW areas. Note width of trails and bike paths.	<i>Updated exhibits and text are included with the submittal.</i>	Resolved



#	Sheet/ Page #	Review Comment	Response (Applicant)	Resolution (City Staff)
8	Site Plan - Streets	An important consideration in determining road geometry is the ability of emergency service vehicles to quickly reach a fire or medical emergency. Does the Roadways permit fire truck access / sufficient turning radius to accommodate these vehicles? Recommend using 'Auto-Turn' or similar program to determine if your design meets any requirements in this regard provided by the Fire Dept.	<i>Roadways will be designed to allow emergency vehicle access throughout. A detailed plan will be provided during Site Plan Review.</i>	Resolved
9	Site Plan - Streets	Sec. 54-821. s.2. Of the Zoning Code states that street connections to adjoining undeveloped tracts shall include a temporary turnaround, in the form of easements or right-of-way, until such time that the adjoining tract is developed, and the temporary turnaround area can be abandoned. Please comply.	<i>The project will comply with Sec. 54-821. s.2. Of the Zoning Code. More detail will be provided during Site Plan Review when road construction and phasing is further refined.</i>	Resolved
10	Site Plan	OCRM critical area: Show label OCRM critical line and/or Wetlands, any required critical line buffers and buffer setbacks. Show the Normal Water Level and buffer zone (show width) for all ponds (if applicable). Show cross-section of wetlands crossing (if applicable) and determine the need for guard rail.	<i>There are no freshwater wetlands on the property and ponds are not being proposed. OCRM critical area is shown on attached exhibit</i>	Resolved
11	Site Plan	Charleston city limit line / any applicable county lines shall be shown.	<i>This is not applicable to the site, which is located wholly within the bounds of the limits of the city of Charleston and Charleston County.</i>	Resolved
12	SWTR	Per Section 3.6.1.i of the City's SDSM, storm drainage pipe shall be placed to minimize the length running under pavement. Where it is necessary for pipe to cross the roadway, it preferably shall be placed at a ninety (90) degree angle and in no case less than forty-five (45) degrees	<i>Thank you for the comment. We will review and comply with the SDSM as construction documents are produced and presented to the City at Site Plan Review.</i>	Resolved



#	Sheet/ Page #	Review Comment	Response (Applicant)	Resolution (City Staff)
13	PUD	Show any proposed major transmission lines and associated proposed easements for electrical, gas, wastewater/sewer, and water.	<i>Proposed easements have not been developed at this stage. They will be presented to the City at Site Plan Review.</i>	Resolved
14	PUD	Provide letter from Charleston Water System (CWS) indicating public water and/or sewer capacity and availability for all of the proposed new development.	<i>Coordination letter attached.</i>	Resolved



# City of Charleston

## Department of Parks

### Technical Review Committee Comments

**PROJECT ID:** PUD2023-000025  
**PROJECT NAME:** UNION PIER  
**ADDRESS:** 190 CONCORD ST.  
**TMS #:** C4590000009

**AGENDA #:** 1  
**DATE:** 05/18/2023  
**REVIEW:** 2<sup>ND</sup> REVIEW  
**REVIEW TYPE:** PUD Master Plan

#### COMMENTS

COMMENTS  NO COMMENTS

1. There are no comments.

Submitted by:

A handwritten signature in blue ink, appearing to read "Rodney H. Porter".

Rodney H. Porter, PLA  
[porterr@charleston-sc.gov](mailto:porterr@charleston-sc.gov)  
City of Charleston, Department of Parks, 823 Meeting Street, Charleston, SC 29403  
843.637.9518 (m), 843.724.7322 (o)



**Department of Stormwater Management**  
Stormwater/MS4 Review Comment Sheet

Page 1 of 2

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**Date:** 03/02/2023, **05/18/2023**  
**To:** South Carolina Ports Authority  
**From:** Ron Bucci, Stormwater Development Manager  
[buccir@charleston-sc.gov](mailto:buccir@charleston-sc.gov) or 843.724.3785  
**cc:** Kinsey Holton, Stormwater Regulatory Practice Leader  
[holtonk@charleston-sc.gov](mailto:holtonk@charleston-sc.gov) or 843.724.3757  
TRC Administrator; File Copy

**Project Name:** Union Pier  
**Project Type:** Planned Unit Development  
**Project TMS #:** C4590000009  
**Project ID #:** PUD2023-000025  
**Submittal Review #:** **2<sup>nd</sup> Review – Comments Provided**

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#	SHEET / PAGE #	COMMENT	RESOLUTION
1	Section 9	<p>Rename this section to “Stormwater and Floodplain Management.” Also rename section 9.4 to “Stormwater Management.”</p> <p><b>The following response was provided:</b></p> <p><i>“This language has been incorporated into the PUD text. “</i></p> <p><b>Please remove ‘Drainage Basin Analysis’ from the title of Section 9.</b></p>	
2	Section 9.4	<p>Please revise this section of the PUD to state that each particular phase or portion of the project will meet the City of Charleston stormwater management requirements in effect at time of submittal of a respective, complete Construction Activity Application (CAA) submittal to the Department of Stormwater Management. With that stated, a Stormwater Management Master Plan could be developed subsequent to the adoption of this PUD, which could account for additional regulatory certainty when designing the public infrastructure improvements. With this, the specific references to standards and requirements from the 2020 SWDSM must be removed from this PUD submittal.</p>	



**Department of Stormwater Management**  
Stormwater/MS4 Review Comment Sheet

Page 2 of 2

#	SHEET / PAGE #	COMMENT	RESOLUTION
		<p>Additionally, remove the reference to the City's Technical Procedure Documents (TPDs) as they are non-regulatory documents intended to provide additional technical clarification to typical issues raised by the design community in meeting the requirements of the SWDSM.</p> <p><b>The following response was provided:</b></p> <p><i>"See revised PUD text in Sec. 9.4. "</i></p> <p><b>Comment not satisfied as this section of the PUD still references the 2020 SWDSM, its specific chapters / requirements, and the TPDs.</b></p> <p><b>Please remove the paragraph on page 19 that starts with 'The project's stormwater management system will be designed to meet....' and all subsequent paragraphs / statements through the end of Section 9.</b></p>	
3	Section 9.4	<p>Please include and/or revise the language in this section to state that not all outfalls will be considered a 'free outfall' due to the fact that the runoff must flow through publically maintained systems prior to reaching the ultimate outfall of the Cooper River.</p> <p><b>The following response was provided:</b></p> <p><i>"The entire project will be master planned with consideration for the ultimate downstream properties. "Free Discharge" is listed as an example of such, but not referenced as the condition of all. "</i></p> <p>Resubmittal did not fully address initial comment. Comment will remain pending until previous comments have been addressed.</p>	

**No new comments issued for the 5/18/2023 TRC meeting resubmittal**



**CITY OF CHARLESTON**  
**DEPARTMENT OF TRAFFIC & TRANSPORTATION TRC REQUIREMENTS**  
**TRANSPORTATION PROJECT MANAGEMENT DIVISION**  
**(843) 724-7372**  
**WallaceJ@charleston-sc.gov**

**SITE:** Union Pier PUD**DATE:** 05/18/2023**TMS #:** Multiple**PLEASE NOTE:** Final approval is contingent upon implementation of comments.**2<sup>nd</sup> Review Comments:** *TRC-PUD2023-000025*

1. Comments to the traffic impact analysis (TIA) provided as separate document.
2. Section 10 – Add language that a phase-specific TIA will be required for any phase of the PUD that meets the City TRC requirements for a TIA. In addition, the master TIA must be updated every five years until the completion of the PUD buildout.
3. No cross-sections provided showing the three-travel lane configuration of East Bay St. and Washington St. contemplated in the TIA. There are concepts for these streets that show two travel lanes and a center turn lane.
4. Hasell St., Laurens St., Society St., Pritchard St. cross sections show no bike lanes but very wide sidewalks. These streets are shown as “Multi-Modal Streets or Multiuse Promenade in Appendix A.9. Are these meant to be multiuse paths to accommodate bicycles?
5. Add PUD language to Section 8 that curb extensions, rectangular rapid flashing beacons (RRFBs), raised crosswalks, raised intersections, and other multimodal friendly features/pedestrian countermeasures will be installed at intersections and mid-block crossings where feasible.
6. Appendix A.9 shows a dedicated bike way adjacent to Washington St. (east side), but the Washington St. cross sections do not show the dedicated bike way. Clarify.
7. Washington St. sections only provide 7' of parking width. Adjust to 8'.
8. Some streets with existing metered parking will be impacted. Is metered parking planned in this development in the future?
9. Concord St. Primary Type 1.A Alternate 02 cross section shows parking adjacent to travel lanes with bike lanes on the outside (parking-protected bike lanes). Concord St. Primary Type 1.B Alternate shows bike lanes adjacent to travel lanes with parking on the outside. Was this done intentionally based on geometry/feedback? Cyclists may prefer parking-protected bike lanes if visibility between vehicular and cycle traffic is maintained at intersections.



**CITY OF CHARLESTON**  
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**TRANSPORTATION PROJECT MANAGEMENT DIVISION**  
**(843) 724-7372**  
**WallaceJ@charleston-sc.gov**



**SITE:** Union Pier PUD Traffic Study

**DATE:** 05/18/2023

**TMS #:** Multiple

PLEASE NOTE: Final approval is contingent upon implementation of comments.

**2<sup>nd</sup> Review Comments:** TRC-PUD2023-000025

The Department of Traffic and Transportation has reviewed the Draft Final edition of the *Union Pier PUD Traffic Impact Analysis* (TIA) (Bihl Engineering, 2023) and has the following comments:

1. Perform an analysis of a concept that utilizes Washington St. as the primary vehicular thoroughfare and converts East Bay St. to a roadway with less emphasis on vehicular traffic and more emphasis on other transportation modes.
2. The TIA does not discuss any kind of phasing or set takedown thresholds for improvements. The TIA should identify when each improvement will be required based on a takedown or phase timeline.
3. Explore opportunities to add turn lanes and maintain two through lanes in each direction where there are existing or proposed combined through/left-turn lanes on four-lane roadway sections (i.e., Calhoun St., East Bay St., Market St., Washington St., etc.). If this is not feasible, explore converting one combined through/left-turn lane to an exclusive left-turn lane while maintaining one through lane.
4. To assist with roadway design, provide estimated Build traffic volumes for the easternmost road (east of Concord St.) on the plan.
5. Ensure that the conceptual layout of the East Bay St./Washington St./Pinckney St. intersection and triangle is feasible.
6. Based on some of the projected side street peak hour volumes, the intersections recommended for future signalization may not meet signal warrants during off-peak hours. Provide a sensitivity analysis. Provide an analysis of unsignalized intersection operations in the Build scenario.
7. Include the following recommendation: "due to their proximity, future signalization of intersections will have to be closely analyzed using microsimulation or other applicable means."
8. Please clarify when the recommended future improvements to the intersection of Concord St. at Charlotte St. will need to be completed.
9. Concerned that queue spillback from the eastbound dual left-turn lanes at the intersection of Calhoun St. at Washington St. will impact operations at the intersection of Calhoun St. at East Bay St. Provide additional analysis.
10. Justify why there is not a left-turn lane from eastbound Calhoun St. onto East Bay St. to give drivers an option of when to turn and split the movement between two intersections.
11. There is potential that the one-way westbound Chapel St. conversion will lead to neighborhood concerns of cut-through and truck traffic.
12. Provide projected average annual daily traffic on roadways projecting cross sectional widening/narrowing to justify widening or reducing overall roadway cross section.
13. Incorporate into the future No Build conditions the westbound approach laneage at the intersection of Calhoun St. at East Bay St. proposed by the South Carolina Department of Transportation (SCDOT) Road Safety Audit (RSA).
14. Provide Lanes, Volumes, Timings Synchro output sheets for Future Build AM and PM conditions.

## **UNION PIER PUD**

### **TRC 2<sup>ND</sup> SUBMITTAL**

### **PLANNING DEPARTMENT REVIEW COMMENTS**

05.17.2023

The following comments are prepared by the Department of Planning, Preservation, and Sustainability. Comments not addressed or requested revisions not submitted in time for the Planning Commission Public Hearing, currently scheduled for June 7th, will be presented to the Planning Commission for its consideration as modifications, should it consider approval of the proposed PUD.

### **PUD TEXT**

*\* denotes similar comment provided at the first TRC review*

### **SECTION 1: RELATIONSHIP TO THE ZONING ORDINANCE**

1. Sec. 1. a. – Remove the first sentence referring to the former comp plan, Century V, “Urban Core”. The updated comp plan, Charleston City Plan (approved 2021), supersedes the former plan. If you choose to keep the sentence for historical perspective, relocate it and reword it so it’s not the lead sentence and doesn’t appear to carry more weight than the updated plan. \*
2. Sec. 1. i. – What governmental services is this referring? If just garbage and stormwater services, please remove. This sentence sounds like the PUD is proposing something more robust. \*

### **SECTION 2: INTRODUCTION AND BACKGROUND INFORMATION**

#### **Section 2.1 Background and Ownership**

3. Sec. 2.1 - Parcel table and map. Where is parcel 7 on the map? Regarding parcels 6 and 7, include a note regarding Dominion’s Limited Agent Authorization form that is included as an appendix. \*

#### **Section 2.4 Affordable and Workforce Housing**

4. Sec. 2.4 – Affordable and Workforce Housing standards are under review by the Department of Housing and Community Development. Separate comments to be provided. \*

### **SECTION 3: LAND USE**

#### **Section 3.2 Net Density and Calculations**

5. Sec. 3.2 – The number of accommodations sleeping units (keyed rooms) must be reduced to a maximum of 300, as stated in earlier discussions. The City is supportive of one Accommodations Use with a maximum of 150 rooms; the balance of allowed hotel rooms shall be developed as hotels with 50 rooms or less. A maximum of one 50-room hotel may be located along the waterfront area; all other hotels shall be located elsewhere on the site. The City would be

receptive to increasing the number of residential units to help offset the reduction of hotel rooms. The distribution of hotel rooms along the waterfront shall not be increased. \*

6. Sec. 3.2 – Revise this entire section as noted below:

### 3.2 Net Density and Calculations (REVISED)

The Union Pier Site is a mixed-use development incorporating flexibility in proposed land uses to accommodate appropriate development over time. The proposed land uses permitted by the Union Pier PUD are defined as Mixed-Use Zoning in Section 4.3. See Appendix A.4, Detailed Land Use Plan for the Master Plan as per Zoning Ordinance requirements. The land use density table below shall set the anticipated net density (Anticipated Area) (Anticipated Units) permitted by the Union Pier PUD and the maximum net density (Max. Area/Units) for each land use:

Zoning	Land Use	Anticipated Area	Anticipated Units	Max. Area/Units
Mixed Use	Retail/Commercial	220,000		*
Mixed Use	Eating and Drinking	50,000		*
Mixed Use	Office	270,000		*
Mixed Use	Accommodations		300 sleeping units	300 sleeping units **
Mixed Use	Residential		1,600 dwelling units ***	*

*\*Retail/commercial, office, eating and drinking, and residential uses can be adjusted via the Land Use Equivalency Matrix maximum of 15%.*

*\*\* Accommodations maximum at 300 units.*

*\*\*\*Any increase in residential units shall require a pro rata increase in obligatory affordable housing units in the final agreement. For example, an increase of 10 residential units would require an increase of 2 affordable housing units.*

Should it be desired for one or more anticipated land use densities to increase (except for the Accommodations net density which shall not increase above 300 sleeping units), a corresponding reduction in densities for other land uses shall be required pursuant to the following Land Use Equivalency Matrix, which is based on the Land Use Equivalency Matrix in the Union Pier PUD Traffic Impact Analysis (Appendix B):

Land Use Equivalency Matrix						
Land Use:		Retail/Commercial	Eating and Drinking	Office	Accommodations	Residential
	Unit	KSF	KSF	KSF	Sleeping Units (S.U.)	Dwelling Unit (D.U.)
Retail/Commercial	1 KSF is equivalent to	1.000	0.376	2.361	5.763	8.718
Eating and Drinking	1 KSF is equivalent to	2.662	1.000	6.285	15.339	23.205
Office	1 KSF is equivalent to	0.424	0.159	1.000	2.441	3.692
Accommodations	1 S.U. is equivalent to	0.174	0.065	0.410	1.000	1.513
Residential	1 D.U. is equivalent to	0.115	0.043	0.271	0.661	1.000

As an example of the Land Use Equivalency Matrix, assume a land use change is proposed from 10,000 sf of retail space to residential dwelling units. Based on the Land Use Equivalency Matrix, 87 additional residential dwelling units (DU) could be substituted in exchange for the reduction of 10,000 sf of retail space. The calculations based on the Land Use Equivalency Matrix are shown below:

$10,000 \text{ sf} / 1,000 \text{ sf} = 10; 10 \times 8.718 = 87$  residential DU substitution for 10,000 sf of retail space.

At or prior to the conveyance of each parcel at the Union Pier Site from the Ports Authority, a restrictive covenant shall be recorded that grants the specific land use density assigned or restricted under each conveyance and the City shall be notified of the same. Unless permitted in such conveyance, density may not, by default, be converted using the Land Use Equivalency Matrix. As land is subdivided and conveyed, an exhibit that clearly identifies the number of residential units, land uses, and/or land use equivalency exchanges shall be provided to the Zoning Administrator and kept on file so uses and units can be tracked in accordance with this PUD document as Union Pier develops.

\*\*\*Approximately forty-four (44) dwelling units per acre, on average, per Upland Area acre of the Site. The Union Pier Site may have greater or fewer dwelling units per acre but shall not exceed 15% deviation from approved maximum. The total number of dwelling units on the Union Pier Site shall be determined by application of the Land Use Equivalency Matrix and Shall not be exceeded. Affordable and workforce housing, as defined in Section 2.4, shall be exempt from the overall dwelling unit density limitations and from the unit counts set forth above. Furthermore, property at the Union Pier Site (as shown on Exhibit \_\_\_\_\_) that remains under the ownership of the Ports Authority

after initial conveyance(s) of the Union Pier Site shall be exempt from the application of the Land Use Density Table while it remains under the ownership of the Ports Authority. *(the property referred to in the sentence above must be specified on an exhibit)*

Uses such as athletic events, performances, special events, public assemblies, and any other uses similar or related thereto, including the venues for providing such uses, shall not be deemed to create and shall be excluded from any area calculations of new, external trips, and shall instead be subject to a traffic management plan upon request by the City and shall be in accordance with City Code Chapter 21 for special events.

## SECTION 4: ZONING CRITERIA

### Section 4.1 Development Standards

7. Sec. 4.1 General comment that requires text to be adjusted throughout this section – Buildings that are five stories or more shall be abutted by sidewalks no less than 10 feet in width. Modified street sections may be approved by the Director of Planning, Preservation & Sustainability in consultation with the Director of Traffic and Transportation at the time of Subdivision Concept Plan review. See also comments provided for the exhibits related to street sections.
8. Sec. 4.1 Development Standards Summary table Maximum Building Height – include a note that buildings that are five stories or more shall be abutted by sidewalks no less than 10 feet in width.
9. Sec. 4.1 – Revise *(new text in italics)* “Internal height district boundaries may be adjusted to abut the final location of constructed rights-of-way as generally depicted in Appendix.... inward or outward to achieve the height district boundaries *as approved by the Zoning Administrator.*”
10. Sec. 4.1 – Revise *(new text in italics)* “Height *in stories* shall be measured from the proposed...” Related to this comment, the City needs to better understand how the grade changes within a block will relate to the number of stories in a building.
11. Sec. 4.1 – Public Service may also need to review encroachments into the public right-of-way. Please discuss this specifically with Public Service. \*

### Section 4.2 Architectural Guidelines

12. Sec. 4.2 – Provide examples, techniques, and/or sketches indicating how the proposed frontage limitation will break down the building massing within a block.
13. Sec. 4.2 – Add the following:
  - a. 4) Where block frontages exceed 200', the block design, or building if full block, shall employ techniques at grade level to prevent long flat flush or non-undulating facades, to maintain a rhythm of solid/void per Charleston streetscapes, and to engage users of the public right-of-way by employing additive and subtractive elements (solids and voids) such as colonnades, portals, recesses and other voids such as pedestrian passageways, walled gardens, outdoor seating areas, green spaces, and fountain plazas. (These tools employ elements which the user experiences three-dimensionally, may envelop a user, or may be interactive. Suggest considering a menu of options.)

- b. 5) At roof level, the building/block design shall employ varying roof heights and/or forms/types and may not simply reflect the height district map maximums. (Add examples, graphics, or chart addressing roof types. This can also be inserted into Appendix G instead.)
  - c. 6) Where building meets grade on sloping street frontage, the building/block shall be designed such that individual building expressions meet grade for entry and access with the exception of residential which shall be allowed stoops to individual unit entries.
  - d. 7) Each block shall express vertically extruded forms and masses that articulate and express a variety of footplate sizes in order to transition with the smaller scale of adjacent neighborhoods. This requires variety in width, height, and/or depth of forms.
14. Sec. 4.2 - Identify corners and locations, such as terminated vistas and focal points, where special attention is an expectation.

#### **Section 4.3 Zoning Districts; Permitted Uses; Hours**

- 15. Sec. 4.3 first paragraph – Why is the PUD referring to permitted uses in the Upper Peninsula?
- 16. Sec. 4.3 – Since proposed uses are similar to GB, are there any limits to hours of operation other than late night entertainment uses? Could all other businesses operate 24/7? \*
- 17. Sec. 4.3 – Establishments for the consumption of alcohol after midnight - these are considered Late Night Entertainment uses. The City is supportive of the conditions in the PUD, however approval of these uses shall go to BZA-Z and follow the City's approval processes due to the controversial nature and the impact they can have on an area.
- 18. Sec. 4.3 – Rooftop uses and related requirements should apply to all properties, not just properties within 500 feet of a residentially zoned district.
- 19. Sec. 4.3 - Subsection 3) for rooftop operational requirements shall be reworded to say "No amplified sound louder than normal conversational tones shall be permitted on the rooftop.

#### **Section 4.5 Accommodations**

- 20. Sec. 4.5 – The number of accommodations sleeping units (keyed rooms) must be reduced to a maximum of 300, as previously stated in earlier discussions. The City is supportive of one Accommodations Use with a maximum of 150 rooms; the balance of allowed hotel rooms shall be developed as hotels with 50 rooms or less. A maximum of one 50-room hotel may be located along the waterfront area; all other hotels shall be located elsewhere on the site. The City would be receptive to increasing the number of residential units to help offset the reduction of hotel rooms. The distribution of hotel rooms along the waterfront shall not be increased. \*
- 21. Sec 4.5 – Provide a note clarifying that if all use conditions are met, approval may be granted by the Zoning Administrator (BZA-Z not required). \*
- 22. Sec. 4.5 Use conditions:
  - a. Condition 2) - recommend changing the minimum room count to 150 for related restaurant requirements.
  - b. Insert condition 8) that ties total food and beverage use area to the total size of accommodations use which reads as follows: "the total square footage of interior and exterior floor area for restaurant and bar space in the proposed accommodations use,

- including restaurant/bar patron use areas, bar areas, kitchen, storage, and bathroom facilities, shall not exceed 12 percent of the total interior, conditioned floor area in the accommodations use, except that each accommodations use shall be permitted to exempt from the calculation of total restaurant floor area one interior, ground floor restaurant tenant space if the total tenant space does not exceed 2,000 square feet, the restaurant tenant does not serve alcoholic beverages, and the exempt restaurant tenant space is clearly labeled with these restrictions on the floor plans submitted with the application for this zoning special exception”
- c. Insert condition 9) pertaining to guest drop off and pick up facility requirements which reads as follows: “the accommodations use proposed guest drop off and pick up area(s) is located outside the public right-of-way and on the property utilized for the accommodations use if the accommodations use contains more than 50 sleeping units, and if 50 sleeping units or less, the proposed guest drop off and pick up area(s) are located outside the public right-of-way if feasible, and the location and design of the guest drop-off and pick-up area(s) has been reviewed by the Department of Traffic and Transportation and determined to be safe and not be an impediment to traffic and that every effort has been made to minimize traffic impacts”
  - d. Insert condition 10) pertaining to restrictions on sharing facilities which reads as follows: “the proposed accommodations use will not share any of its buildings, structures, facilities, or operations with another accommodations use”
  - e. Insert condition 11) pertaining to payment of fee for workforce housing account which reads as follows: “to assist in providing affordable housing opportunities for its employees, the applicant for the accommodations use commits to contribute to the City of Charleston Affordable/Workforce Housing Account a fee payable upon the issuance of a Certificate of Occupancy, calculated as follows: \$5.10 per square foot (or any updated value approved by Council and included in the Zoning Ordinance) of area used for sleeping units and the hallways adjacent to sleeping units, stairwells and elevators”.

## **SECTION 5: OPEN SPACE AND BUFFERS**

- 23. Sec. 5.1 – Development of the open space should not be tied to the timing of TIF performance, rather open space must be delivered prior to or at the same as adjacent development. Provide an open space phasing plan or parameters/thresholds for the delivery of open space to ensure that open space is completed with or prior to adjacent development. Open space should not be the last component to be completed. \*
- 24. Sec. 5.1 Provide acreage for pier removed, area over critical area
- 25. Sec. 5.1. - Provide a chart indicating acreage of each park/open space type. Identify which open space types are counting towards open space high ground area.
- 26. Sec. 5.2 – Union Pier Waterfront Park must include a playground. The possibility of playgrounds in other locations is also encouraged.
- 27. Sec. 5.2 - Please make sure park labels on the exhibit are consistent with the names/types described in Sec. 5.2.

28. Sec. 5.2 – The Pedestrian Alleys are listed under the open space section however they are not shown on the Open Space exhibit. Are they open space or not? Adjust for consistency.

#### **SECTION 8: RIGHT-OF-WAY STANDARDS**

29. Sec. 8 –Based on concerns related to the traffic study and input from the Department of Traffic and Transportation, provide four lanes on Washington Street. The concept of four lanes on Washington Street is preferable over the three lanes proposed on Washington and East Bay Streets.
30. Sec. 8 - Based on the traffic study, provide a list of development thresholds and the related necessary right-of-way/traffic improvements that must be made. The correlation of development and timing of street improvements needs to be clear.
  - a. For example: 1) East Bay connections/intersection configurations must be tied to certain development phases, buildup percentage, and/or # units. 2) Washington St improvements/connections must be completed with X% development build out.
31. Sec. 8 – See also detailed comments related to Street Sections in Appendix A.

#### **SECTION 9: DRAINAGE BASIN ANALYSIS; STORMWATER AND FLOODPLAIN MANAGEMENT**

32. Sec. 9.1 – Will the proposed barrier protection infrastructure take the property out of the LiMWA, and if so, what is the process for remapping the LiMWA? What if the LiMWA is not remapped? How will that affect the proposed PUD (building heights, building/street relationships, street activation, underground parking, etc)?

#### **SECTION 12: LETTERS OF COORDINATION**

33. Sec. 12 - Will there be a Dominion power substation within the Union Pier area?
34. Sec. 12 – Provide coordination letter from Charleston County School District about school availability.

#### **APPENDICES/EXHIBITS**

#### **APPENDIX A**

35. All exhibits (except street sections) – Show and label critical line. Currently the critical line is labeled on some exhibits as “approximate”, but hasn’t the location been verified per the survey exhibit? Revise note as necessary.
36. Exhibits A.3 and A.6 – Adjust/deepen the color for Open Space (High Ground); it is difficult to discern when the document is printed.
37. Exhibits A.4 and A.5 – The number of accommodations sleeping units (keyed rooms) must be reduced to a maximum of 300, as previously stated in earlier discussions. The City is supportive of one Accommodations Use with a maximum of 150 rooms; the balance of allowed hotel rooms shall be developed as hotels with 50 rooms or less. A maximum of one 50-room hotel may be located along the waterfront area; all other hotels shall be located elsewhere on the site. The City would be receptive to increasing the number of residential units to help offset the

reduction of hotel rooms. The distribution of hotel rooms along the waterfront shall not be increased. \*

38. Exhibit A.5 – Height District Plan.

- a. Provide a new exhibit that models the proposed height district massing as it relates to newly shown proposed grade changes. This exhibit should also include existing buildings. The proposed grade changes provide much needed information, but we now have additional questions.
- b. See the redlined exhibit below and the previously provided height revisions recommended by Staff.
  - i. To respect the context (height) of existing neighboring buildings to the south and north:
    - For the blocks closest to the Customs House – the height of new buildings shall not exceed the parapet height of the Customs House (see map below). Include a notation to this effect.
    - For the blocks closest to the Anson House, The Gadsden, and Williams Terrace – height of new buildings shall not exceed the parapet height of the aforementioned buildings (see map below). Include a notation to this effect.
  - ii. Revise the heights for blocks closest to the water as shown on the map below and in previous staff comments.
  - iii. Revise the heights for the block closest to East Bay and Market as shown on the map below and in previous staff comments.
  - iv. The Planning Dept may have additional comments regarding proposed building heights once additional information is provided on the site section exhibits.
  - v. Provide a chart that indicates the distribution of heights/stories over the entire developable footprint (i.e., 3 stories = X%, 6 stories = X%).

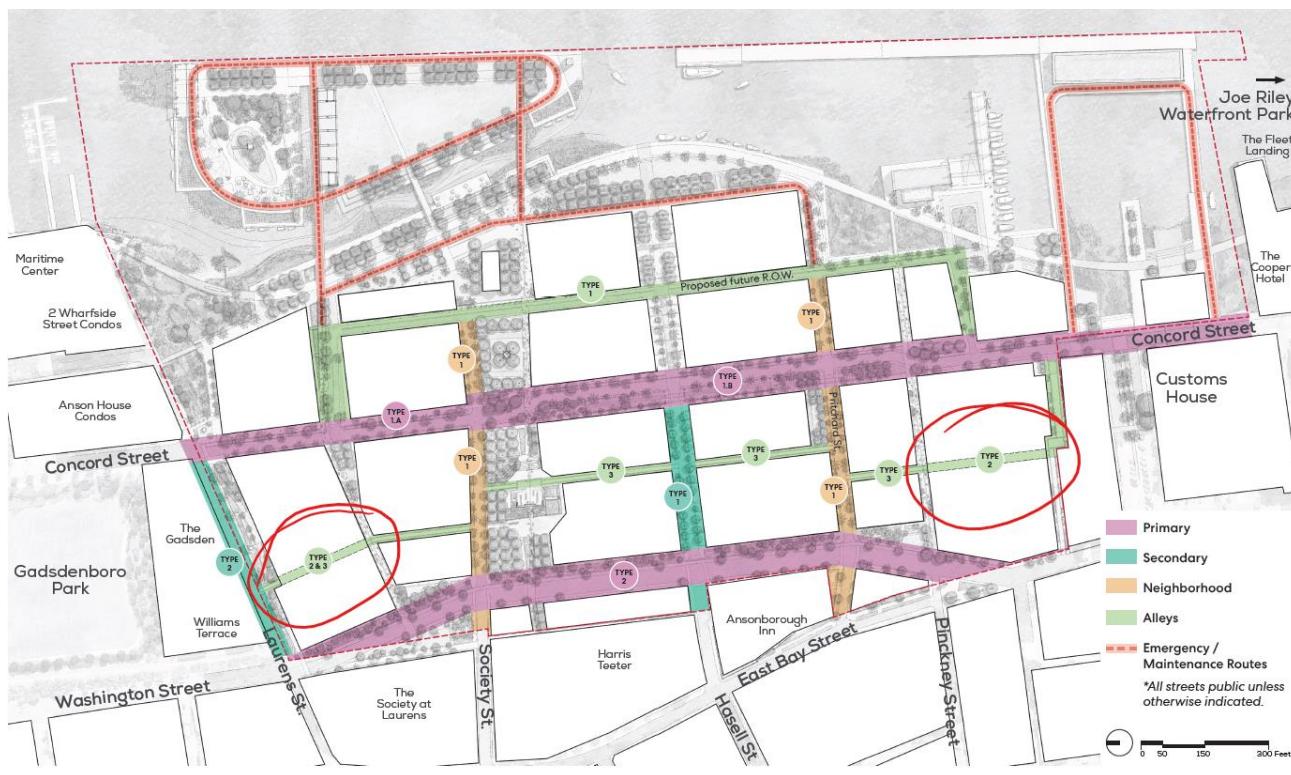


39. Exhibit A.6 – A playground must be provided on the Waterfront Park/Island Park. Please make sure park labels on the exhibit are consistent with the names/types described in Sec. 5.2.
40. Exhibit A.6 - Pedestrian Alleys are listed under the open space section in the PUD text however they are not shown on the Open Space exhibit. Are they open space or not? Please adjust for consistency.
41. Exhibits A.7 and A.8 – Clarify whether the Green Infrastructure Alleys are pedestrian or vehicular alleys (i.e., note in legend). It gets a bit challenging switching back and forth between exhibits to make sure we fully comprehend what is being proposed.
42. Exhibit 8.3 - Indicate proposed building heights on grading plan.
43. Exhibit 8.3 - Overlay contour lines.
44. Exhibits 8.4-8.6 – Provide a conceptual on-the-ground 3D massing view from adjacent streets. Revise existing exhibits to include more information -- proposed building heights combined with the proposed grade changes -- so that one can understand how build-out might look with regards to height and massing on land that is being elevated. \*
45. Exhibits 8.4-8.6 - Show building heights on the site section exhibits and other exhibits as requested.
46. Exhibit A.10 – Identify the Pedestrian Alleys. They are included in the street cross sections. Are these alleys also open space?
47. Exhibit A.10 – It would be extremely helpful if this exhibit or a supporting exhibit could show street types and building heights.

48. Exhibit A.10 and all corresponding street sections – Considering the development goals, the number of visitors and citizens that will come to the site for shopping, dining, events, visiting the waterfront and park spaces, etc. – does the site and street sections provide enough publicly accessible on-street parking?
- How many on-street parking spaces could be provided based on the proposed sections/plan?
  - Provide an exhibit that demonstrates the location of on street parking and the possible location of parking garages.
  - Distinguish between the functions of streets and alleys. Are some alleys strictly to service buildings rather than accommodate pedestrian travel?
  - Structures that are five stories or more shall be abutted by sidewalks no less than 10 feet in width. Modified street sections may be approved by the Director of Planning, Preservation & Sustainability in consultation with the Director of Traffic and Transportation at the time of Subdivision Concept Plan review. Include a note in this section to this effect.
49. Exhibit A.10 – Regarding Type 1 street labeled “Proposed future ROW” and serving the blocks that abut the waterfront, this street needs to be revised to accommodate on-street parking on at least one side, street trees, and wider sidewalks (min. 10 ft each side). This should not be an alley, and an alley condition should not be the terminus for Society and Pritchard Streets. It should be more accommodating to visitors to the waterfront park, adjacent buildings, service vehicles, etc. As an alley, it feels as though it is privatizing the waterfront park and making it less accessible to citizens and visitors. Right-of-way should be a minimum 50’.
50. Exhibit A.11 Primary Type 1.A (Concord St) Alternate 02 – Regarding sidewalk width: Buildings that are five stories or more shall be abutted by sidewalks no less than 10 feet in width. Modified street sections may be approved by the Director of Planning, Preservation & Sustainability in consultation with the Director of Traffic and Transportation at the time of Subdivision Concept Plan review. Include a note in this section to this effect. See also comments provided for the exhibits related to street sections.
51. Exhibit A.11 Primary Type 2 (Washington St) Alternate – Regarding sidewalk width: Buildings that are five stories or more shall be abutted by sidewalks no less than 10 feet in width. Modified street sections may be approved by the Director of Planning, Preservation & Sustainability in consultation with the Director of Traffic and Transportation at the time of Subdivision Concept Plan review. Include a note in this section to this effect. See also comments provided for the exhibits related to street sections.
52. Exhibit A.11 Neighborhood Type 1 (Society & Pritchard) – Regarding sidewalk width: Buildings that are five stories or more shall be abutted by sidewalks no less than 10 feet in width. Modified street sections may be approved by the Director of Planning, Preservation & Sustainability in consultation with the Director of Traffic and Transportation at the time of Subdivision Concept Plan review. Include a note in this section to this effect. See also comments provided for the exhibits related to street sections. Add on-street parking.
53. Exhibit A.11 Alleys - Simplify proposed alley types to two types – one with sidewalks and one without (based on function of alley). If sidewalks are provided on alleys, sidewalks must be

minimum 4 feet wide. Modified street sections may be approved by the Director of Planning, Preservation & Sustainability in consultation with the Director of Traffic and Transportation at the time of Subdivision Concept Plan review. It may also be feasible that alley lighting may be provided on abutting buildings, which can be determined with subdivision concept plan or building site plans.

54. Alleys incorporated in garages/buildings: Define and illustrate the condition shown on dashed line blocks as shown in the exhibit below. Provide a third alley type and section for any alley that is incorporated with a multi-level parking facility or building (dashed line alleys) to understand the juxtaposition of building and alley.



55. Exhibit A.12 - Provide travel direction arrows on all one-way streets (existing and proposed). On the proposed exhibit, indicate which intersections would require modifications. Include new traffic signals, stop conditions. Part of East Bay Street is hatched, provide more detail on this condition.

#### APPENDIX B TRAFFIC IMPACT ANALYSIS

56. See related comments provided for Section 8 above and all right-of-way exhibits.
57. Break out Land Use Equivalency Matrix so that it is either incorporated into Sec. 3.2 or is a sub exhibit (i.e., B.1) so that it is easier to locate in the PUD. It needs to be more accessible and

easier to review because, in addition to traffic impacts, the matrix also relates to Zoning and tracking densities and uses as the property is subdivided and conveyed.

58. In future submittals, reorder exhibits to place traffic study at end of submittal to make other information/appendices easier to locate and review.

## **APPENDIX F**

### **Appendix F, Section 10. Streetscape. Green Infrastructure Strategies**

59. 9.C- Primary Type 1A- The flush curb is the only alteration from exhibit 9.D (Alternative). Consolidate to 9.D only.
60. See PUD text and Appendix A comments above regarding building height/sidewalk width and the potential to modify street sections as related to the items below:
  - a. 9.D - Sidewalk must be a minimum 10' in width, separate from the 7' Bioretention zone.
  - b. 9.E - Sidewalk must be a minimum 10' in width, separate from the 3' Bioretention zone.
  - c. 9.F The 14' multi use path is also labeled as 'Stormwater Garden" define. If this is to be a boardwalk or similar, the sidewalk area should be a minimum of 10' in width.
  - d. 9.J - Sidewalk must be a minimum of 10' in width, separate from the 5' Bioretention zone.

## **APPENDIX G \***

61. Revise "Guideline/Guidelines" to "Principle/Principles".
62. At preamble section:
  - a. omit "intended" and "suggested"
  - b. insert "Union Pier Architectural Principles shall be guided by City zoning ordinances, the Secretary of Interior Standards, and BAR Policy Statements and Guidelines."
  - c. strike "with the exception of Civic buildings, which shall have no design guidelines". Staff may be open to other appropriate notations providing flexibility as once suggested by DPZ but this needs to be reviewed.
63. Guideline 5 - Could instead decrease the amount of sky visible to a pedestrian and/or increase watershed to the pedestrian. While Staff supports the inclusion of a requirement for sloped roofs, the purpose should be adjusted to instead be to provide interesting rooflines, various roof heights, to properly reside in the Charleston context, or to prevent monotonous flat-topped extruded footprints.
64. Create new guidelines or revise existing to address the following:
  - a. "Wherever possible, green resilient building practices should be employed. Utilize Charleston Rises for advisory practices."
  - b. "Charleston blocks are diverse...A means of achieving this is variety in building scales, styles, and typologies. Each block shall express vertically extruded forms and masses that articulate and express a variety of floorplate sizes in order to transition with the smaller scale of adjacent neighborhoods. This requires variety in width, height, and/or depth of forms." Or similar.

- c. "A Charleston tradition is the articulation of rhythm in streetscapes...A means of achieving this is through rhythmic repetition of fenestration, articulation of forms, and an authentic relationship of solids and voids at the street." Or similar.
  - d. "A Charleston tradition is the emphasis on terminated vistas and corners which has established the City's aesthetic identity...A means of achieving this principle is through careful study of viewsheds and reflecting these important vistas in the built environment". Or similar.
65. Chart - Revise "Generally Easier to Approve < ---- > More Difficult to Approve" to "Recommended and Generally Easier to Approve < ---- > Highly Discouraged and More Difficult to Approve".



# CHARLESTON FIRE DEPARTMENT

## Fire Marshal Division



### Permit Application & Plan Review Comments

Project		Project Status	
Name:	Union Pier PUD	Status:	<b>Approved w/ comments</b>
Address:	190 Concord St	Received:	May 8, 2023
Type:	Site Plan Review	Reviewed:	May 17, 2023
Submittal:	Revised submittal review	Revised plans required:	No
Permit #:	PUD2023-000025	Written response required:	No
Project Contact		Fire Plans Reviewer	
Name:	Randolph Lowell	Name:	Rick Fluegge
Company:	South Carolina Ports Authority	Title:	Deputy Fire Marshal
Phone:		Phone:	(843)724-5893
Applicant's Email:	rowell@burr.com	Reviewer's Email:	fluegger@charleston-sc.gov

Thank you for your recent submittal. We look forward to working with you throughout this project in order to ensure the safety of our citizens and our emergency responders.

Please submit a written response to the following comments and annotate these corrections on the revised drawings. Response letters must list explanations in the same order as the comments appear on this review letter. Permits will not be issued until all items have been reviewed and work shall not commence until the issuance of the permit. Please be sure to address all items on the review in order to expedite the review process.

#### General:

1. The April 25, 2023, response to the Union Pier PUD TRC - Fire Department review comments indicate the needed information will be provided as part of the typical TRC review process. This information will be needed to ensure the project meets the applicable sections of the South Carolina Fire Code (SCFC) that will be in effect at the time of plan submission and should not be construed as to granting "blanket" approval of the entire project at this time.

Plan review by this office does not include a check of every item and does not relieve the owner, contractor, or designer of meeting all code requirements. Issuance of a permit shall not be construed as authority to violate local, State, or Federal rules or regulations. Work commencing prior to permitting may result in double permit fees and/or fines.