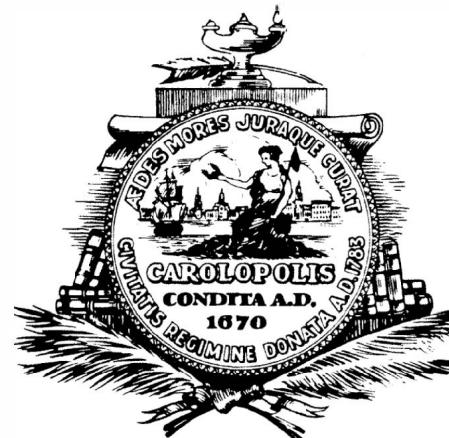


PROPOSED PLANS FOR THE CITY OF CHARLESTON

TRC-SUB2023-000249



APPROVED FOR RIGHT-OF-WAY
AND
EASEMENT CONSTRUCTION

The Department of Development Services of the City of Charleston has reviewed these plans and determined that they meet or exceed engineering standards of principles and practice and guidelines as set forth in the City of Charleston Ordinance, Standards, and other applicable requirements and are therefore approved for construction of the road and drainage systems as shown herein. No variances from the Ordinance, Standards or other applicable requirements are approved unless they are specifically noted in the Conditions of Approval below. The City has no objection to the installation of the water and sewer systems as shown herein, subject to approval by SCDES and the appropriate utility agency. Any changes or modifications to these plans must be requested in writing to the Technical Review Committee and / or appropriate review agency. Written approval must be obtained prior to implementing requested changes or modifications.

 11/24/2025
Signature Date
Conditions of Approval:

Hydraulic Design Reference for these plans is the:
2009
Edition of SCDOT's "Requirements for
Hydraulic Design Studies"

Design Reference for these plans is the:
3B

These Plans Were Designed Using "3R" Principles From the 2021 Roadway Design Manual

NPDES PERMIT INFORMATION

Project Area _____ Acres(s) _____

Approximate Location of Roadway is

Latitude	32°51'33.74" N
Longitude	79°54'35.04" W

Hydraulic and NPDES Design
provided by:

	RIGHT-OF-WAY		CONSTRUCTION	
	INITIAL	DATE	INITIAL	DATE
ROAD				
STRUCTURES				

THE INITIALS ABOVE DO NOT RELIEVE THE ENGINEER OF RECORD OF THE RESPONSIBILITY TO DESIGN THIS PROJECT IN ACCORDANCE WITH ALL APPLICABLE CRITERIA.

For Right Of Way Acquisition:

Consultant Engineer of Record

Date

Regional Production Engineer

Date

ENGINEER OF RECORD

SOUTH CAROLINA

PARRISH
AND
PARTNERS, LLC

NO. 4784

CONSTRUCTION PLANS

SOUTH CAROLINA

LICENSED PROFESSIONAL ENGINEER

Don Alexander, No. 35050:1395

CONSTRUCTION PLANS

TON D. ALEXANDER

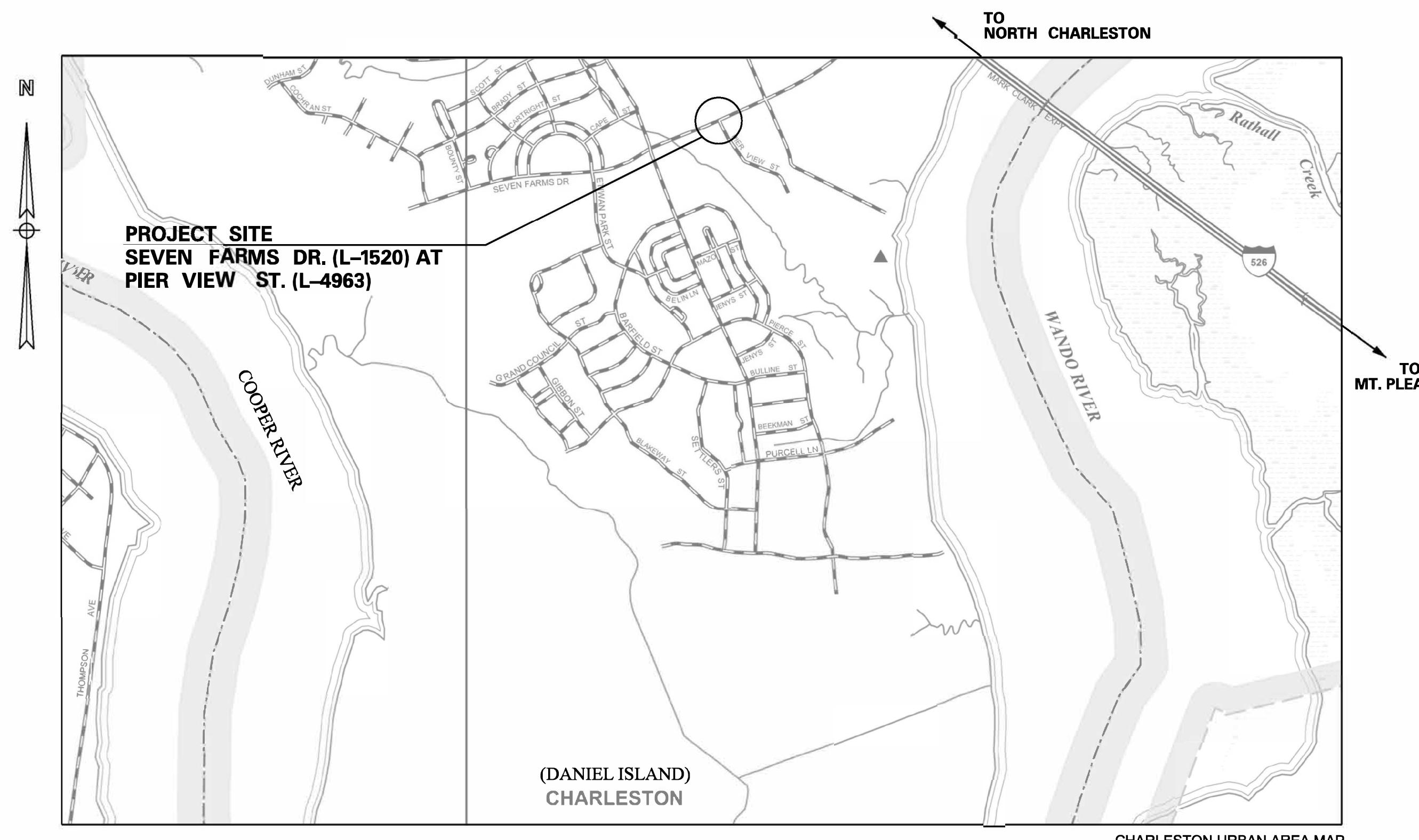
-05'00'

CONSTRUCTION PLANS

INDEX OF SHEETS		
<u>SHEET #</u>	<u>DESCRIPTION</u>	<u>SHEET SUBTOT</u>
1	TITLE SHEET	1
10F1	EXISTING CONDITIONS SURVEY	1
2	SUMMARY OF EST. QUANTITIES	1
3	TYPICAL SECTIONS	1
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8	EROSION CONTROL SHEET	1
TC1	TRAFFIC CONTROL SHEET	1
TC2	TRAFFIC CONTROL SHEET	1
PM1	PAVEMENT MARKING & SIGNING	1
TS1	RRFB DETAIL SHEET	1
EC1	EROSION CONTROL DETAILS	1
EC2	EROSION CONTROL NOTES	1
SD1	INTERSECTION SIGHT DIST.	1
<hr/>		TOTAL SHEETS
		17

TOTAL SHEETS

SEVEN FARMS DRIVE PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION OF PIER VIEW STREET



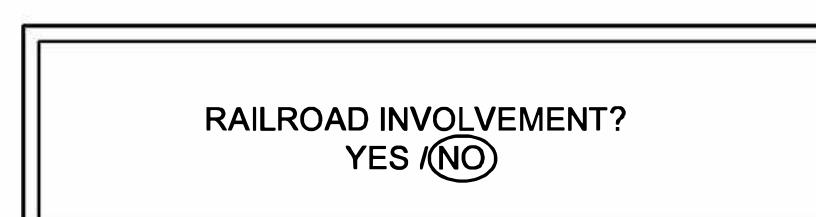
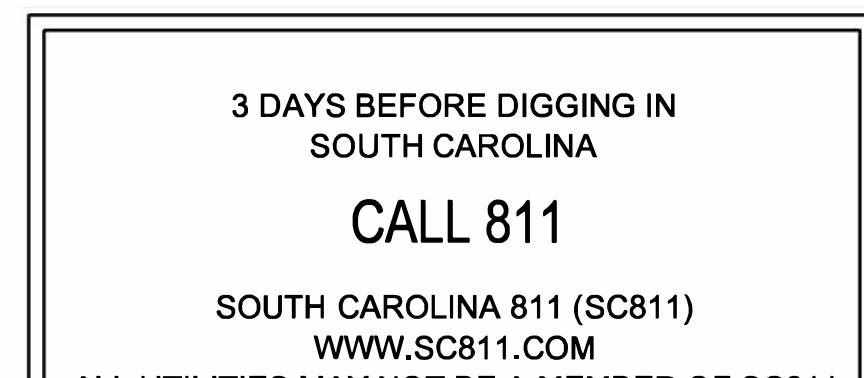
LAYOUT

SCALE 1 INCH = 0.25 MILES

SEVEN FARMS DR. (L-1520)		TOTAL
NET LENGTH OF ROADWAY	0.050	0.050
NET LENGTH OF BRIDGES	0.000	0.000
NET LENGTH OF PROJECT	0.050	0.050
LENGTH OF EXCEPTIONS	0.000	0.022
GROSS LENGTH OF PROJECT	0.050	0.050

NOTE: EXCEPT AS MAY OTHERWISE BE SPECIFIED ON THE PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIALS AND WORKMANSHIP ON THIS PROJECT SHALL CONFORM TO THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (2007 EDITION) AND THE STANDARD DRAWINGS FOR ROAD CONSTRUCTION IN EFFECT AT THE TIME OF LETTING.

THE PUBLIC RIGHT-OF-WAY MUST REMAIN ADA COMPLIANT DURING ALL CONSTRUCTION ACTIVITIES OR AN ALTERNATE ROUTE MUST BE PROVIDED.



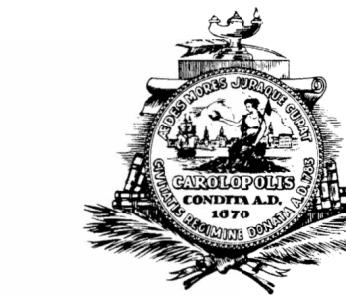
SUMMARY OF ESTIMATED QUANTITIES



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PARRISH & PARTNERS



CITY OF CHARLESTON

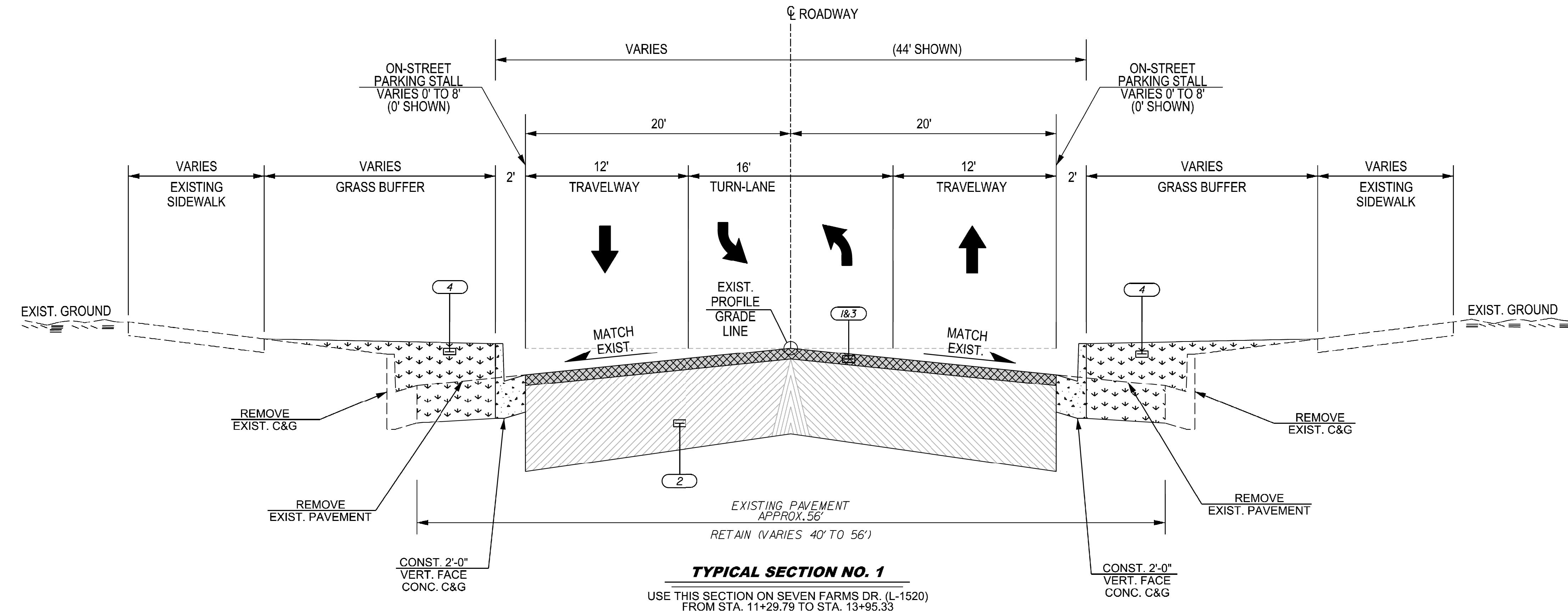
SEVEN FARMS DR. (L-1520) PEDESTRIAN IMPROVEMENTS

SUMMARY OF ESTIMATED QUANTITIES

SCALE: NTS

DWG. NO.

TYPICAL SECTION OF IMPROVEMENT CITY OF CHARLESTON



NOTES:

- CURB RAMPS AND DETECTABLE WARNING SURFACE ARE TO BE CONSTRUCTED IN ACCORDANCE WITH STANDARD DRAWING NOS. 720-929-01 & 720-929-02.
- SEE STANDARD DRAWING NOS. 720-105-01 & 720-105-02 FOR DETAILS OF CONCRETE CURB & GUTTER (2.0').
- GUTTER SLOPE SHALL BE ACHIEVED IN ACCORDANCE WITH STANDARD DRAWING NO. 150-205-00.
- SEE PLANS FOR LOCATIONS OF SIDEWALK AND CURB & GUTTER.
- SEE PLANS FOR EXCEPTIONS TO TYPICAL SECTIONS.
- SEE PLANS FOR LOCATIONS OF EXISTING PAVEMENT REMOVAL

ROAD OR ROUTE NO.	FUNCTIONAL CLASS	DESIGN SPEED			PAVEMENT DESIGN ENGINEER
		MPH	FROM STA.	TO STA.	
L-1520	URBAN MAJOR COLLECTOR	25	11+29.79	13+95.33	
L-4963	URBAN LOCAL RESIDENTIAL STREET	25	30+00.00	31+49.88	

PAVEMENT LEGEND

- 1 HOT MIX ASPHALT SURFACE COURSE (TYPE B) (200 PSY)
- 2 EXISTING PAVEMENT (RETAIN) - MILL 2"
- 3 MILL EXISTING PAVEMENT 2.0"
- 4 FILL AND SOD



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REV. NO. BY DATE DESCRIPTION OF REVISION

PARRISH & PARTNERS



CITY OF CHARLESTON

**SEVEN FARMS DR. (L-1520)
PEDESTRIAN IMPROVEMENTS**

TYPICAL SECTIONS

SCALE: NTS

DWG. NO. 3

GENERAL NOTES

1. WORK IN THIS PROJECT SHALL CONFORM TO THESE PLANS, THE LATEST EDITIONS OF THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION (SCDOT) STANDARD DRAWINGS, THE SOUTH CAROLINA DEPARTMENT OF HEALTH AND ENVIRONMENTAL CONTROL (SCDHEC), THE SITE WORK SPECIFICATIONS, AND GENERAL DESIGN STANDARDS. IN THE EVENT OF CONFLICT BETWEEN ANY OF THESE STANDARDS, SPECIFICATIONS, OR PLANS, THE MOST STRINGENT SHALL GOVERN UNLESS OTHERWISE NOTED IN THESE PLANS.
2. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ALL JOBSITE SAFETY, INCLUDING BUT NOT LIMITED TO TRENCH SAFETY. DURING ALL PHASES OF CONSTRUCTION.
3. THE LOCATION AND SIZE OF EXISTING UTILITIES AS SHOWN ARE APPROXIMATE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR HORIZONTALLY AND VERTICALLY LOCATING AND PROTECTING ALL PUBLIC OR PRIVATE UTILITIES (SHOWN OR NOT SHOWN) WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE. AT LEAST 72 HOURS PRIOR TO ANY DEMOLITION, GRADING, OR CONSTRUCTION ACTIVITY, THE CONTRACTOR SHALL NOTIFY THE SOUTH CAROLINA PALMETTO UTILITY PROTECTION SERVICE (PUPS) AT 1-888-721-7877 FOR PROPER IDENTIFICATION OF EXISTING UTILITIES WITHIN THE SITE.
4. THE CONTRACTOR SHALL SALVAGE AND PROTECT ALL EXISTING POWER POLES, SIGNS, MANHOLES, TELEPHONE RISERS, WATER VALVES, ETC. DURING ALL CONSTRUCTION PHASES. THE CONTRACTOR SHALL REPAIR, AT HIS OWN EXPENSE, ANY EXISTING UTILITIES DAMAGED DURING CONSTRUCTION.
5. TRAFFIC CONTROL ON PUBLIC STREETS IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE IN CONFORMANCE WITH TRAFFIC CONTROL DETAILS LOCATED IN THE SCDOT STANDARD DRAWINGS AND AS FURTHER DIRECTED BY CITY AND STATE INSPECTORS.
6. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SPECIFICATIONS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS OR SPECIFICATIONS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES, OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
7. CONTRACTOR IS RESPONSIBLE FOR VERIFYING OR OBTAINING ALL REQUIRED PERMITS AND APPROVALS PRIOR TO COMMENCING CONSTRUCTION, AND FOR COMPLYING WITH ALL PERMIT CONDITIONS, INCLUDING MONITORING, TESTING, AND REPORTING.
8. THE FRAMES AND COVERS OF ALL EXISTING AND PROPOSED DRAINAGE, SANITARY SEWER, WATER MAIN, GAS, AND WIRE UTILITY STRUCTURES SHALL BE ADJUSTED TO MATCH PROPOSED FINISHED ELEVATIONS AND SLOPES.
9. ROADWAYS MUST BE CAPABLE OF SUPPORTING FIRE APPARATUS DURING CONSTRUCTION.
10. SURVEY PROVIDED BY: ESP ASSOCIATES, INC.
11. ALL MARKINGS WITHIN SCDOT ROW SHALL BE APPROVED PERMANENT PAVEMENT MARKINGS PER SECTIONS 625, 626, OR 627 WITHIN THE SCDOT STANDARD SPECS. FOR HIGHWAY CONSTRUCTION.
12. CONTRACTOR SHALL BE RESPONSIBLE FOR UTILIZING ALL APPLICABLE AND CURRENT SCDOT STANDARD DRAWINGS INCLUDING, BUT NOT LIMITED TO, THE DRAWINGS INCLUDED OR REFERENCED WITHIN THESE PLANS AND THE APPROVED PERMIT PACKAGE.
13. UPON SUBSTANTIAL PROJECT COMPLETION, CONTRACTOR TO CLEAR EXISTING CULVERTS/PIPES, CATCH BASINS, AND DITCHES ALONG FRONTEAGE AND DOWNSTREAM AS NECESSARY TO ACHIEVE POSITIVE DRAINAGE.
14. ALL PROPOSED OR RELOCATED SIGNAGE SHALL BE PLACED OR REPLACED IN ACCORDANCE WITH SECTION 650-000 AND INSTALLED ON SCDOT APPROVED BREAKAWAY SIGN SUPPORTS AS DETAILED IN SECTION 654-000 IN THE SCDOT STANDARD DRAWINGS.
15. CONTRACTOR TO REMOVE AND REPLACE DAMAGED SIDEWALK OR CURB & GUTTER ALONG PROPERTY FRONTEAGE AS NECESSARY AND/OR AS SPECIFIED BY CITY STAFF.
16. SIDEWALK AND CURB & GUTTER TO BE REMOVED AND REPLACED FROM JOINT TO JOINT.
17. ANY PROPOSED SIDEWALK WITHIN THE ROW SHALL NOT EXCEED 2% CROSS SLOPE PER ADA GUIDELINES. ALL RAMPS SHALL NOT EXCEED 8.33% FOR EVERY SCENARIO.
18. IN PRESENCE OF SIDEWALK OR OTHER ADA PATHWAY, ACCESS(ES) TO PROVIDE A MINIMUM OF 3' PEDESTRIAN PATH OF NO GREATER CROSS SLOPE THAN 2% ACROSS THROAT OF DRIVEWAY.
19. PAVEMENT TRANSITION BETWEEN MILLED SURFACE AT BUTT JOINTS SHALL BE TIED-IN SMOOTHLY AND SHALL BE FREE OF "BUMPS".
20. INVERTS OF THE TIE-IN LOCATION TO THE EXISTING STORM DRAINAGE SYSTEM ARE UNKNOWN. CONTRACTOR TO VERIFY INVERT ELEVATION AND PAVEMENT THICKNESS AND PROVIDE INFORMATION TO ENGINEER OF RECORD. ENGINEER MAY ADJUST PROPOSED PIPE INVERTS (LOWER) TO ENSURE AT LEAST ONE FOOT OF COVER BELOW THE BOTTOM OF THE PAVEMENT.
21. CONTRACTOR WILL ENSURE THAT ALL ASPECTS OF CONSTRUCTION WILL MEET OR EXCEED THE REQUIREMENTS OF THE CITY OF CHARLESTON STORMWATER MANUAL UNLESS OTHERWISE NOTED.
22. CONTRACTOR SHALL PROVIDE TREE PROTECTION PRIOR TO BEGINNING ALL CONSTRUCTION ACTIVITIES ON ALL ADJACENT TREES. TREE PROTECTION SHALL CONSIST OF 2' (MIN) HIGH, ORANGE, UV-RESISTANT, HIGH TENSILE STRENGTH POLYETHYLENE FABRIC WITH REINFORCEMENT OR APPROVED EQUAL. FENCE SHALL HAVE A 8' (MIN) CLEARANCE OR AS OTHERWISE DIRECTED BY THE CITY ARBORIST. NO STORAGE OF CONSTRUCTION MATERIALS SHALL BE PLACED INSIDE FENCE. CITY ARBORIST SHALL BE CONSULTED IF ROOTS, BRANCH TRIMMING, OR OTHER MODIFICATION IS NEEDED TO ANY TREE OR LANDSCAPING IN GENERAL.
23. CONTRACTOR SHALL IMMEDIATELY CONTACT THE OWNER OR ENGINEER OF RECORD IF ANY OBJECT WITH POTENTIAL HISTORIC OR CULTURAL RESOURCE SIGNIFICANCE IS FOUND DURING CONSTRUCTION ACTIVITIES.

PROPOSED CURB ELEVATIONS

1. THE CONTRACTOR SHALL SET AND ADJUST PROPOSED CURB ELEVATIONS AS NECESSARY TO ENSURE PROPER LONGITUDINAL GRADE FOR DRAINAGE.
2. WHERE PROPOSED CURB & GUTTER TIES TO EXISTING CURB OR CURB & GUTTER, A TRANSITION OF 10' SHALL BE MADE TO CONFORM TO THE EXISTING HEIGHTS AND SHAPES.

GRADING

1. THE CONTRACTOR IS RESPONSIBLE FOR THE LOCATION OF ALL UTILITIES. 72 HOURS PRIOR TO ANY DEMOLITION, GRADING, OR CONSTRUCTION ACTIVITY THE CONTRACTOR SHALL NOTIFY THE SOUTH CAROLINA PALMETTO UTILITY PROTECTION SERVICE (PUPS) AT 1-888-721-7877 FOR PROPER IDENTIFICATION OF EXISTING UTILITIES WITHIN THE PROJECT SITE.
2. CONTRACTOR IS RESPONSIBLE FOR THE REPAIR AND REPLACEMENT OF ANY UTILITIES DAMAGED DURING CONSTRUCTION. DAMAGED ITEMS SHALL BE REPAIRED TO AT LEAST THE QUALITY OF WORKMANSHIP FOUND IN THE ORIGINAL ITEM.
3. ALL AREAS SHALL BE GRADED FOR POSITIVE DRAINAGE.
4. CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ALL EROSION CONTROL DEVICES SHOWN ON THE APPROVED PLANS FOR THE DURATION OF CONSTRUCTION OR UNTIL FINAL INSPECTION AND APPROVAL.
5. IN ORDER TO ENSURE PROPER DRAINAGE, MAINTAIN A MINIMUM 0.30% SLOPE ON THE CURB.
6. ALL MATERIALS USED FOR FILL OR BACK-FILL SHALL BE FREE OF WOOD, ROOTS, ROCKS, BOULDERS OR ANY OTHER NON-COMPACTABLE SOIL TYPE MATERIAL. UNSATISFACTORY MATERIALS ALSO INCLUDE MAN-MADE FILLS, REFUSE, DEBRIS DERIVED FROM ANY SOURCE.
7. ALL PAVEMENT SUB GRADES SHALL BE SCARIFIED TO A DEPTH OF 8 INCHES AND COMPACTED TO A MINIMUM DENSITY OF 100% OF ASTM D-1557 DENSITY AT OPTIMUM MOISTURE CONTENT UNLESS OTHERWISE SHOWN ON THE CONSTRUCTION PLANS OR AS DIRECTED BY THE GEOTECHNICAL ENGINEER. FILL SHALL BE PLACED AND COMPACTED IN MAXIMUM 8" LIFTS. IN AREAS WHERE ROCK IS ENCOUNTERED AT FINAL SUB GRADE ELEVATION, THE EXPOSED ROCK SHALL BE TOPPED WITH A LEVELING COURSE OF SANDY CLAY OR CLAYEY SAND (P.I. BETWEEN 4 AND 15) AS NEEDED TO PROVIDE A SMOOTH SURFACE FOR PAVING.
8. THE CONTRACTOR SHALL CLEAR AND GRUB THE SITE AND PLACE, COMPACT, AND MOISTURE CONDITION ALL FILL PER THE PROJECT ENGINEER'S SPECIFICATIONS. THE FILL MATERIAL TO BE USED SHALL BE APPROVED BY THE OWNER OR ENGINEER PRIOR TO PLACEMENT.
9. ALL CONTOURS ARE IN REFERENCE TO THE BENCHMARK AND MUST BE VERIFIED BY THE CONTRACTOR PRIOR TO GROUND BREAKING.
10. THE LIMITS OF CLEARING SHOWN ON THE GRADING AND EROSION CONTROL PLAN ARE BASED ON THE APPROXIMATE CUT AND FILL SLOPE LIMITS, OR OTHER GRADING REQUIREMENTS.
11. LIMITS OF PROPOSED SLOPES ARE INDICATED IN THE PLANS, DETAILS AND STANDARD DRAWINGS. THE MAXIMUM FILL SLOPE SHALL NOT EXCEED A 4:1 (HORIZONTAL TO VERTICAL) UNLESS DESIGNATED BY THE ENGINEER. A CUT SLOPE OF 2:1 MAXIMUM WILL BE USED ONLY AS DIRECTED BY THE ENGINEER.
12. ALL EXCESS FILL, EXCAVATED MATERIAL AND DEBRIS FROM CLEARING OPERATIONS SHALL BE DISPOSED OF IN A LEGAL MANNER.

GENERAL CONDITIONS

1. ALL PROOF ROLLS WILL BE CONDUCTED BETWEEN THE HOURS OF 8:00 A.M. AND 4:00 P.M. MONDAY - FRIDAY.
2. PROOF ROLL WILL BE REDONE IF THE PROJECT IS RAINED ON BEFORE ANY CONCRETE, STONE, OR ASPHALT IS INSTALLED.
3. THE CONTRACTOR OR THIRD PARTY INSPECTOR WILL SCHEDULE PROOF ROLL. THE THIRD PARTY INSPECTOR OR HIS OR HER REPRESENTATIVE SHOULD BE PRESENT AT ALL PROOF ROLLS.
4. UNDERCUTTING UNSUITABLE MATERIAL IS ACCEPTABLE, USING ONLY APPROVED COMPACTABLE, SUITABLE FILL. UNDERCUTTING USING EXTRA STONE AS FILL IS NOT ACCEPTABLE.
5. ANY FIELD CHANGES MUST BE APPROVED BY BOTH THE ENGINEER OR RECORD AND CITY OF CHARLESTON.
6. IF A GEOTECHNICAL/SOILS ENGINEER IS USED ON THE PROJECT, HE OR SHE SHOULD BE PRESENT AT ALL PROOF ROLLS.
7. CITY OF CHARLESTON WILL HAVE THE FINAL DECISION ON ALL PROOF ROLLS.

EXISTING SANITARY SEWER AND WATER LINE

THE CONTRACTOR SHALL USE CARE WHEN WORKING AROUND SANITARY SEWERS AND WATER LINES. SHOULD THE CONTRACTOR DAMAGE EXISTING SEWER OR WATER LINES, THEY SHALL IMMEDIATELY REPLACE THE LINE AT HIS EXPENSE WITH DUCTILE IRON PIPE TO THE UTILITY OWNER'S SATISFACTION. THE CONTRACTOR SHALL REPLACE SANITARY SEWER AND/OR WATER LINE WITH A MINIMUM TWENTY FOOT SECTION OF DUCTILE IRON PIPE WHEN DRAINAGE PIPE COMES WITHIN TWO FEET OF SAID LINES, VERTICALLY OR HORIZONTALLY.

SAWCUTS

THE CONTRACTOR SHALL SAWCUT EXISTING ASPHALT AND/OR CONCRETE SURFACES PRIOR TO REMOVAL UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ALL SAWCUT OF PAVEMENT, SIDEWALKS, ETC., SHALL BE PER SCDOT SPECIFICATIONS AND PERPENDICULAR TO FLOW OF TRAFFIC. PAVEMENT SAWCUTS SHALL BE ACCOMMODATE FULL LANE WIDTH REPAIR/RE-PAVING.

UTILITIES

UTILITIES ARE ILLUSTRATED FOR INFORMATION PURPOSES ONLY. SCDOT AND ENGINEER WILL NOT BE HELD ACCOUNTABLE FOR THE ACCURACY OF UTILITY LOCATIONS, SIZES, DEPTHS OR FOR COMPLETENESS OF UTILITY INFORMATION. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS WHOSE FACILITIES WILL BE AFFECTED TO DETERMINE UTILITY LOCATIONS. THE CONTRACTOR SHALL PROTECT ALL UTILITIES FROM DAMAGE CAUSED BY HIS OPERATIONS OR THOSE OF HIS AGENTS. THE CONTRACTOR SHALL HOLD THE CLIENT HARMLESS FOR ANY THIRD-PARTY INCONVENIENCE CREATED BY WORK OF HIS OWN FORCES OR THAT OF HIS AGENTS.

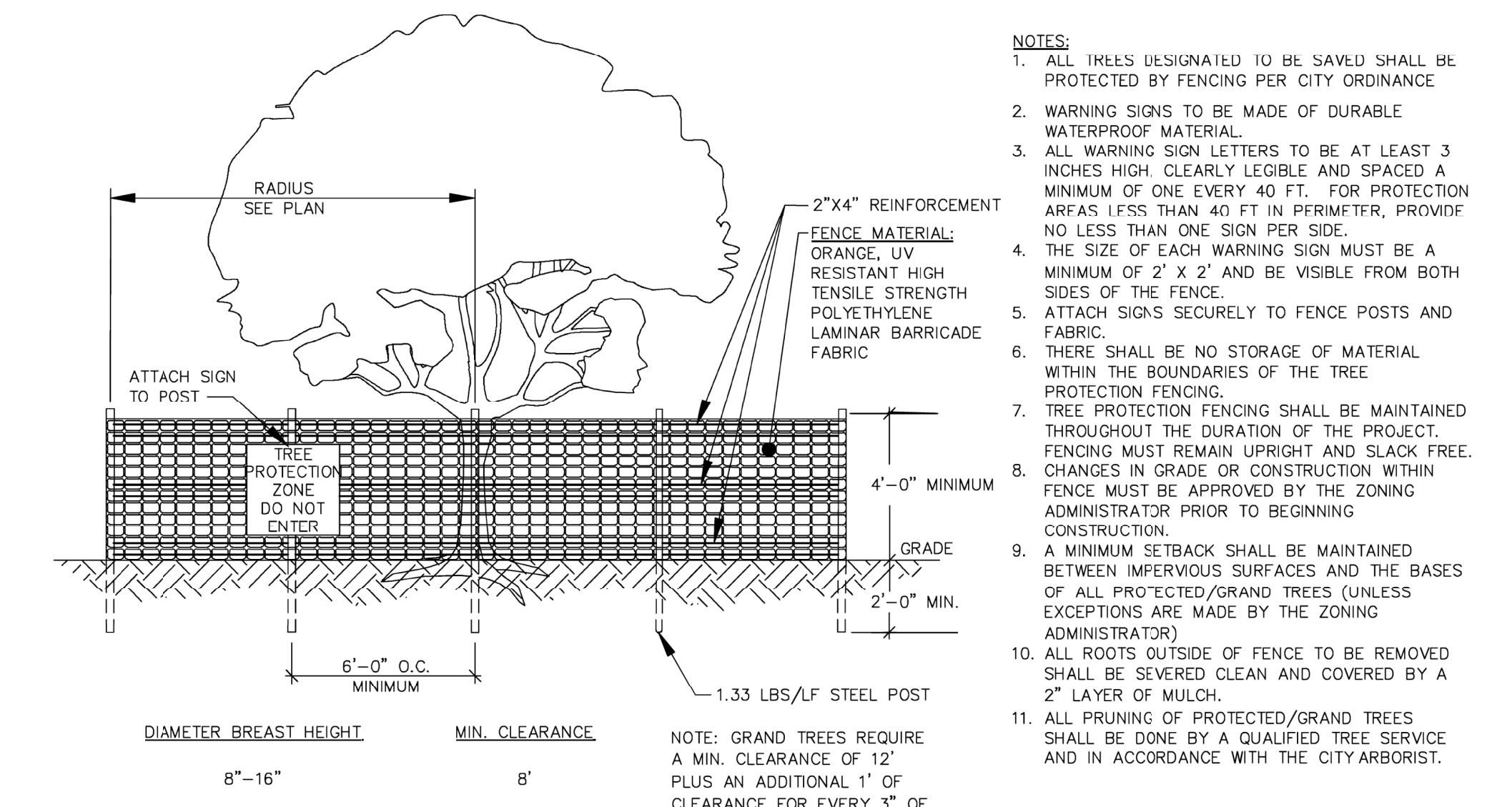
AS NEEDED, THE CONTRACTOR SHALL ADJUST/RELOCATE THE SANITARY SEWER AND WATER LINES ONLY. ALL OTHER ADJUSTMENTS/RELOCATIONS WILL BE PERFORMED BY THE VARIOUS UTILITY OWNERS. THE CONTRACTOR SHALL COORDINATE WORK WITH UTILITY OWNERS SO AS NOT TO ADVERSELY AFFECT THE PROJECT SCHEDULE.

- FOR UTILITY LOCATIONS, CALL PUPS AT 1-888-721-7877

- FOR LOCATIONS OF UTILITIES NOT MEMBERS OF SOUTH CAROLINA ONE-CALL, CONTACT THE ENGINEER OF RECORD.

THE CONTRACTOR SHALL ADJUST ALL WATER VALVES, WATER METER BOXES, AND WATER VAULTS TO FINISHED GRADE. WATER METERS LOCATED IN SIDEWALKS OR CONCRETE DRIVEWAYS SHALL BE INSTALLED WITHIN CONCRETE BOXES.

GAS LINES WILL BE ADJUSTED/RELOCATED AS NEEDED BY GAS UTILITY. A REPRESENTATIVE OF GAS UTILITY MUST BE PRESENT JUST PRIOR TO AND DURING CONSTRUCTION NEAR EXISTING GAS LINES. THE CONTRACTOR SHALL CONTACT GAS UTILITY AT LEAST TWO WEEKS PRIOR TO CONSTRUCTION.



TREE PROTECTION

CITY OF CHARLESTON

SEVEN FARMS DR. (L-1520)
PEDESTRIAN IMPROVEMENTS

GENERAL CONSTRUCTION NOTES

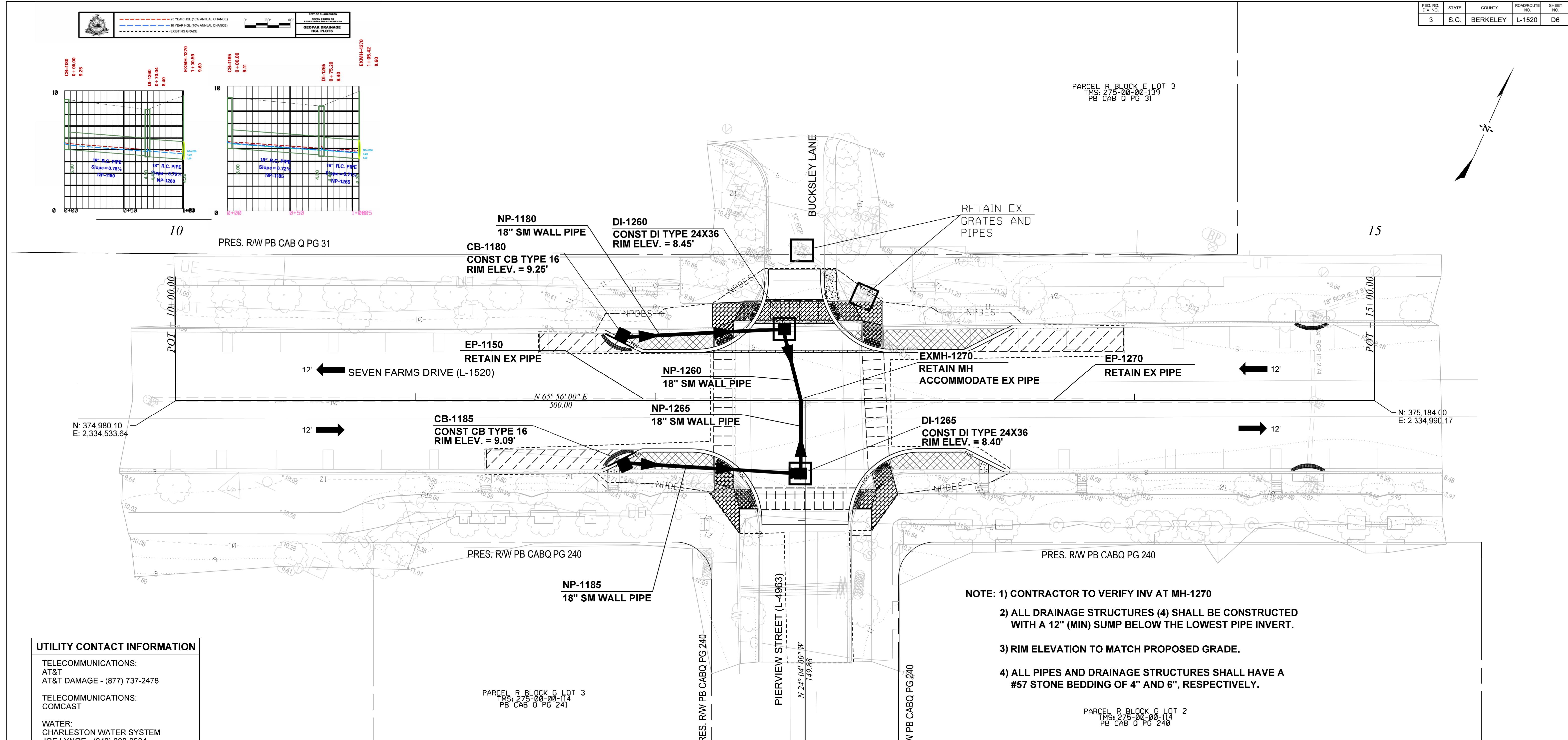
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PARRISH & PARTNERS





UTILITY CONTACT INFORMATION

TELECOMMUNICATIONS:
AT&T
AT&T DAMAGE - (877) 737-2478

TELECOMMUNICATIONS:
COMCAST

WATER:
CHARLESTON WATER SYSTEM
JOE LYNGE - (843) 308-8284

TELECOMMUNICATIONS:
HOME TELEPHONE
JESSICA LITCHFIELD - (843) 761-9113

ELECTRIC:
DOMINION ENERGY
NICK SPROUSE
UTILIQUEST PR - (803) 791-3929

GAS:
DOMINION ENERGY
NICK SPROUSE
UTILIQUEST PR - (803) 791-3929



PIPE DATA SHEET



CITY OF CHARLESTON

SEVEN FARMS DR. (L-1520) EQUESTRIAN IMPROVEMENTS

DRAINAGE PLAN SHEET

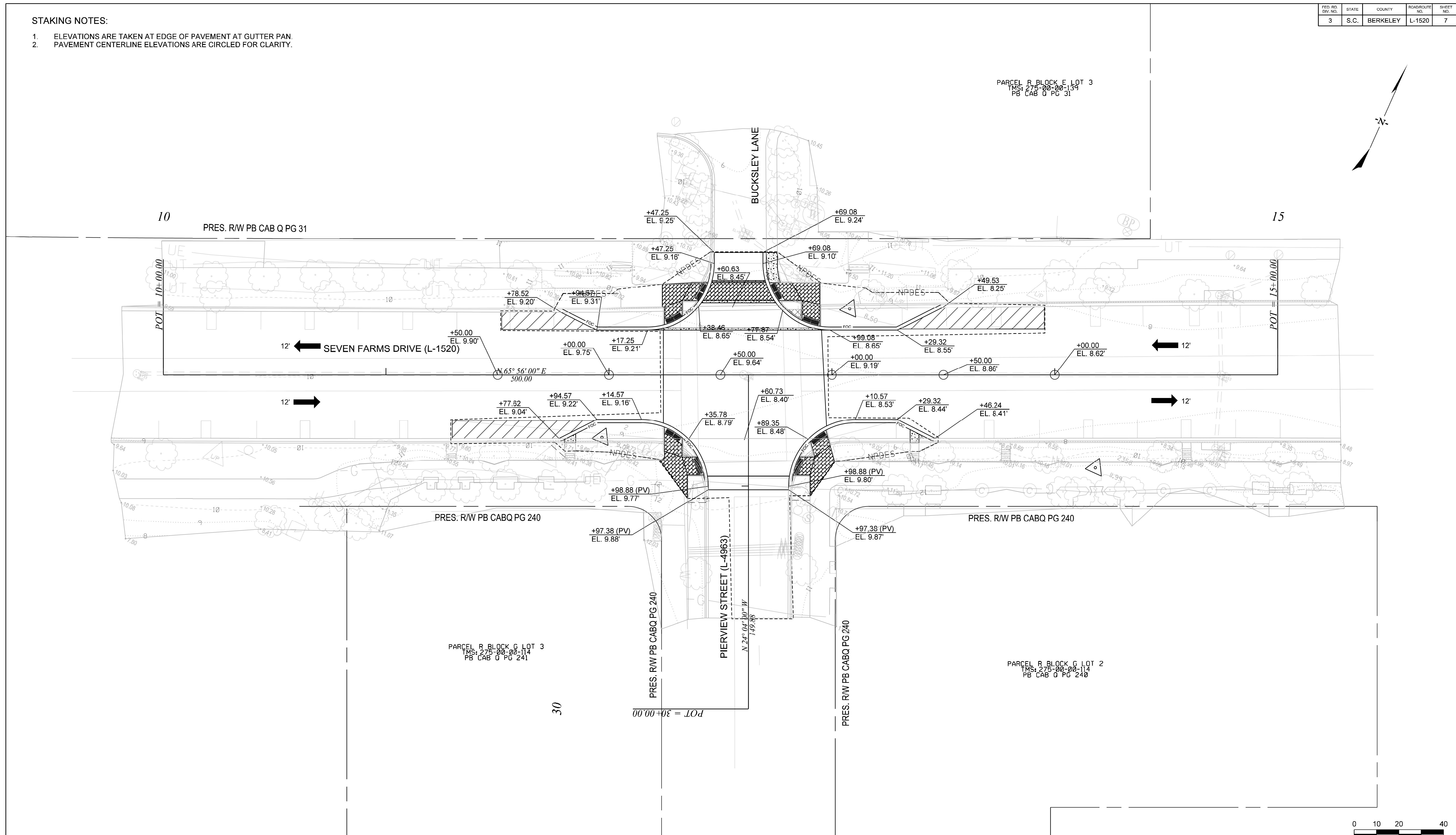
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STAKING NOTES

1. ELEVATIONS ARE TAKEN AT EDGE OF PAVEMENT AT GUTTER PANS
2. PAVEMENT CENTERLINE ELEVATIONS ARE CIRCLED FOR CLARITY

FED. RD. DIV. NO.	STATE	COUNTY	ROAD/ROUTE NO.	SHEET NO.
3	S.C.	BERKELEY	L-1520	7

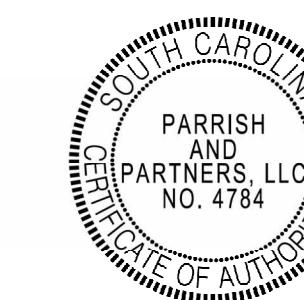


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PARRISH & PARTNER



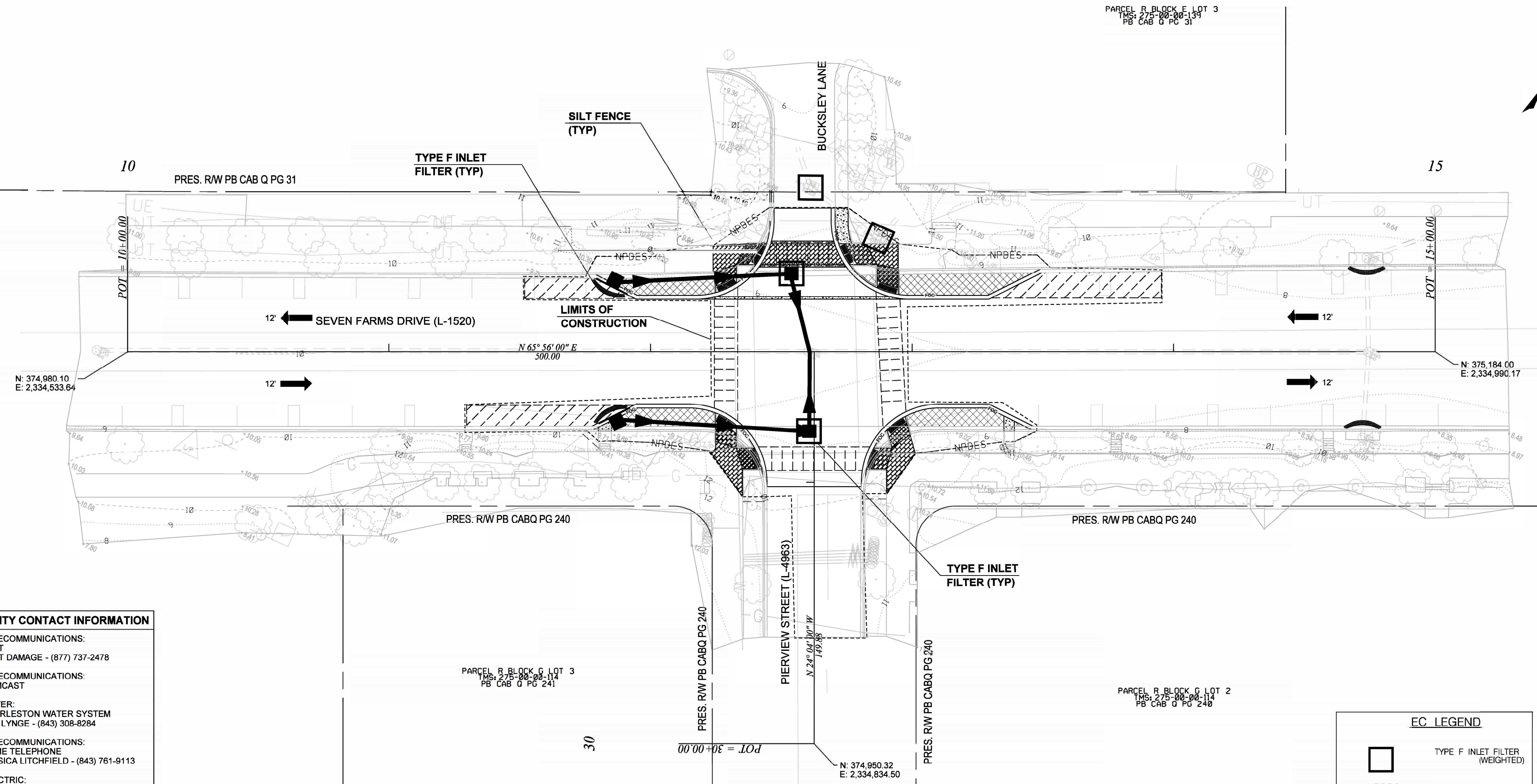
CITY OF CHARLESTON

SEVEN FARMS DR. (L-1520) EQUESTRIAN IMPROVEMENTS

MAKING ELEVATIONS SHEET

CALE: 1" = 20'

DWG. NO.



pw:\parish-pw.bentley.com:parish-pw\Documents\Surface Transportation\City of Charleston\Seven Farms at Pier View\Roadway\Sheets\SevenFarms_SHEET-08-EROSIONCONTROLPLAN.dgn
DAlexander

UTILITY CONTACT INFORMATION

**TELECOMMUNICATIONS:
AT&T
AT&T DAMAGE - (877) 737-2478**

TELECOMMUNICATIONS: COMCAST

WATER:
CHARLESTON WATER SYSTEM
JOE LYNCE (843) 208-8284

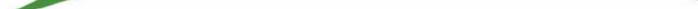
**TELECOMMUNICATIONS:
HOME TELEPHONE
JESSICA LITCHFIELD - (843) 761-9113**

ELECTRIC:
DOMINION ENERGY
NICK SPROUSE
UTILIQUEST PR - (803) 791-3929

GAS:
DOMINION ENERGY
NICK SPROUSE
UTILIQUEST PR - (803) 791-3929



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PARRISH & PARTNERS



CITY OF CHARLESTON

VEN F FARMS DR. (L-1520) ESTRIAN IMPROVEMENTS

EROSION CONTROL SHEET

CALE: 1" = 20'

DWG. NO.

<u>EC LEGEND</u>	
	TYPE F INLET FILTER (WEIGHTED)
NPDES —	SILT FENCE
	TYPE F INLET TUBES

DWG. NO.

GENERAL NOTES

- ALL SIGN LOCATIONS ARE TO BE MEASURED FROM THE WORK AREA. WORK LIMITS FOR THE PROJECT WILL BE DETERMINED BY THE ENGINEER AND AS INDICATED IN THE CONTRACT. CONSTRUCTION SIGNING SHALL BE INSTALLED PRIOR TO CONSTRUCTION BEGINNING ON ALL ROUTES.
- PORTABLE SIGN SUPPORT MOUNTED ADVANCE SIGNS ARE TO BE INSTALLED NO LESS THAN 6 FEET FROM THE EDGE OF AN ADJACENT TRAVEL LANE WHEN A PAVED SHOULDER IS PRESENT AND NO LESS THAN 4 FEET FROM THE EDGE OF AN ADJACENT TRAVEL LANE WHEN THE SHOULDER IS EARTH AND NO PAVED SHOULDER EXISTS. IN THOSE AREAS WITH CURB & GUTTER, INSTALL THE PORTABLE SIGN SUPPORT MOUNTED ADVANCE SIGNS NO LESS THAN 2 FEET FROM THE FACE OF THE CURB.
- SPACINGS INDICATED ARE FOR NORMAL CONDITIONS; ADJUSTMENTS MAY BE REQUIRED DUE TO HORIZONTAL AND/OR VERTICAL ALIGNMENTS OR OTHER SIGHT DISTANCE RESTRICTIONS.
- ALL SIGNS MOUNTED ON PORTABLE SIGN SUPPORTS SHALL HAVE A MINIMUM MOUNTING HEIGHT OF 5 FEET FROM THE GROUND TO THE BOTTOM OF THE SIGN. ALL SIGNS MOUNTED ON GROUND MOUNTED U-CHANNEL POSTS SHALL HAVE A MINIMUM MOUNTING HEIGHT OF 7 FEET FROM THE GROUND TO THE BOTTOM OF THE SIGN. MOUNT ALL SIGNS SUCH THAT THEY ARE STRAIGHT AND LEVEL AND THE FACE OF THE SIGNS ARE PERPENDICULAR TO THE SURFACE OF THE ROADWAY.
- REFLECTORIZE ALL ORANGE ADVANCE SIGNS AND ALL ORANGE AREAS OF MULTI-COLORED ADVANCE SIGN WITH A FLUORESCENT ORANGE COLORED PRISMATIC REFLECTIVE SHEETING WHERE THE SIGNS ARE TO BE ORANGE. ALSO, REFLECTORIZE ALL WHITE ADVANCE SIGNS AND ALL WHITE AREAS OF MULTI-COLORED ADVANCE SIGN WITH A WHITE COLORED PRISMATIC REFLECTIVE SHEETING WHERE THE SIGNS ARE TO BE WHITE.
- ALL TRAFFIC CONTROL DEVICES, INCLUDING 36" CONES, OVERSIZED CONES, PORTABLE PLASTIC DRUMS, TYPE II BARRICADES, PORTABLE SIGN SUPPORTS, SIGN SUBSTRATUMS, TRUCK MOUNTED ATTENUATORS, BREAKAWAY SYSTEMS FOR GROUND MOUNTED SUPPORTS, ETC., SHALL COMPLY WITH ALL NCHRP REPORT 350 REQUIREMENTS AND SHALL BE APPROVED BY THE DEPARTMENT. ALL APPROVED TRAFFIC CONTROL DEVICES ARE INCLUDED ON THE "APPROVED PRODUCTS LIST FRO TRAFFIC CONTROL DEVICES IN WORK ZONES". THIS LIST MAY BE ACCESSED ON THE DEPARTMENT'S WEB SITE AT: WWW.SCDOT.ORG.
- REFLECTORIZE ALL DRUMS AND BARRICADES WITH A TYPE III HIGH INTENSITY REFLECTIVE SHEETING UNLESS OTHERWISE DIRECTED BY THE DEPARTMENT.
- THE CONTRACTOR SHALL CONDUCT THE WORK IN SUCH A MANNER SO AS NOT TO ENCROACH ONTO THE LANE ADJACENT TO THE CLOSED LANE. THE CONTRACTOR SHALL PLACE, MAINTAIN, AND ADJUST THE TRAFFIC CONTROL DEVICES AS NECESSARY.
- WORK IS TO BE CONDUCTED ON ONE SIDE OF SEVEN FARMS DRIVE AT A TIME.
- CONTRACTOR TO COORDINATE WITH CITY OF CHARLESTON STAFF DURING CONSTRUCTION TO ENSURE THAT PEDESTRIAN DETOURS FROM ADJACENT PROJECTS ARE ACCOMMODATED PROPERLY.
- CONTRACTOR TO APPLY ALL NECESSARY SCDOT 600 SERIES STANDARD DRAWINGS (TRAFFIC CONTROL) AS NECESSARY TO COMPLETE CONSTRUCTION ACTIVITIES.

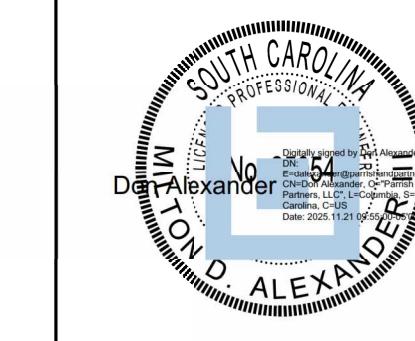
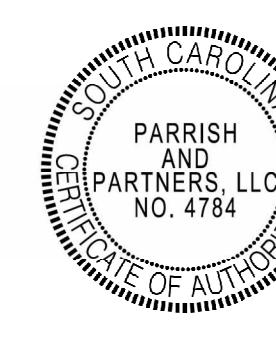
CITY STANDARD NOTES

- SIGHT DISTANCE VISIBILITY AT ALL EXITS AND/OR INTERSECTIONS WILL BE MAINTAINED IN ACCORDANCE WITH SCDOT's ACCESS AND ROADSIDE MANAGEMENT STANDARDS MANUAL.
- ALL TRAFFIC CONTROL DEVICES WILL BE TO MUTCD STANDARDS (MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES).
- IF TRAFFIC SIGNS OR MARKINGS WITHIN THE RIGHT-OF-WAY ARE IMPACTED, RELOCATION OF THESE ITEMS MUST BE COORDINATED WITH TRAFFIC AND TRANSPORTATION PRIOR TO CONSTRUCTION.
- IF THE STREET IS BLOCKED OR IMPACTED DURING CONSTRUCTION AT ANY TIME FOR ANY REASON A STREET BLOCKING PERMIT WILL BE REQUIRED. COORDINATE WITH TRAFFIC AND TRANSPORTATION PRIOR TO CONSTRUCTION.
- NO CONSTRUCTION PARKING OR STAGING WILL BE PERMITTED WITHIN THE RIGHT-OF-WAY WITHOUT PRIOR AUTHORIZATION BY TRAFFIC AND TRANSPORTATION.
- LANE CLOSURES OF ANY TYPE OR DURATION WITHIN THE RIGHT-OF-WAY MUST BE APPROVED BY TRAFFIC AND TRANSPORTATION WELL IN ADVANCE OF THE OCCURRENCE. COORDINATE WITH TRAFFIC AND TRANSPORTATION PRIOR TO CONSTRUCTION.
- CONSTRUCTION AND DEMOLITION TRAFFIC MUST AVOID RESIDENTIAL STREETS AT ALL TIMES UNLESS THERE ARE NO ALTERNATIVE ROUTES. IF IMPACTS TO RESIDENTIAL STREETS ARE ANTICIPATED, THE CONTRACTOR SHOULD CONTACT TRAFFIC AND TRANSPORTATION PRIOR TO USING THE ROUTE.
- REMOVAL OR CHANGES TO PARKING METERS AND/OR PARKING METERED SPACES OR OTHER ON-STREET PARKING/LOADING ZONES MUST BE COORDINATED WITH TRAFFIC AND TRANSPORTATION PRIOR TO CONSTRUCTION.
- IF METERED (OR NON-METERED) PARKING SPACES THAT ARE NOT APPROVED FOR REMOVAL ARE BLOCKED OR IMPACTED DURING DEMOLITION OR CONSTRUCTION AT ANY TIME FOR ANY REASON A METER BAC PERMIT (OR CONSTRUCTION PARKING PERMIT) WILL BE REQUIRED. COORDINATE WITH TRAFFIC AND TRANSPORTATION PRIOR TO CONSTRUCTION.
- IF TRAFFIC SIGNALS OR TRAFFIC SIGNAL EQUIPMENT IS IMPACTED IN ANY WAY, SHOW EXISTING AND PROPOSED CHANGES. CHANGES OR IMPACTS TO TRAFFIC SIGNAL ITEMS MUST BE COORDINATED WITH TRAFFIC AND TRANSPORTATION PRIOR TO CONSTRUCTION.
- IMPROVEMENTS TO THE RIGHT-OF-WAY PAVEMENT MARKINGS MAY BE REQUIRED. CROSSWALKS, CENTERLINES, PAVEMENT MARKING ARROWS, BIKE AND PEDESTRIAN MARKINGS, ETC. ADJACENT TO THE SITE MAY BE REQUIRED TO BE REPLACED IF DAMAGED.
- CONTRACTOR WILL REPLACE/REPAIR ANY CURBS, SIDEWALKS, PAVEMENTS, ETC.. DAMAGED DURING CONSTRUCTION.



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PARRISH & PARTNERS



CITY OF CHARLESTON
SEVEN FARMS DR. (L-1520)
PEDESTRIAN IMPROVEMENTS
TRAFFIC CONTROL PLAN
SCALE: NTS
DWG. NO. TC1

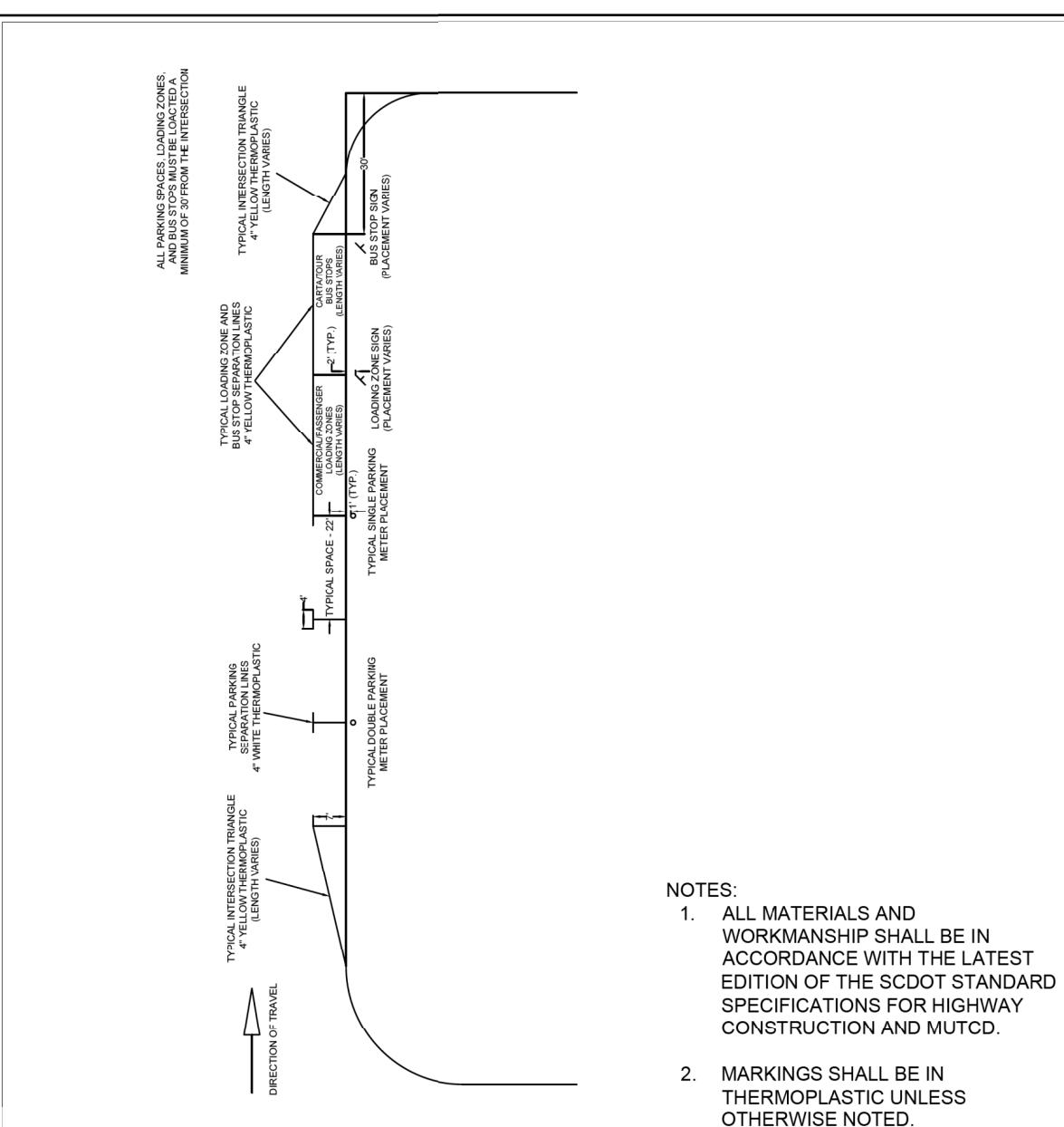
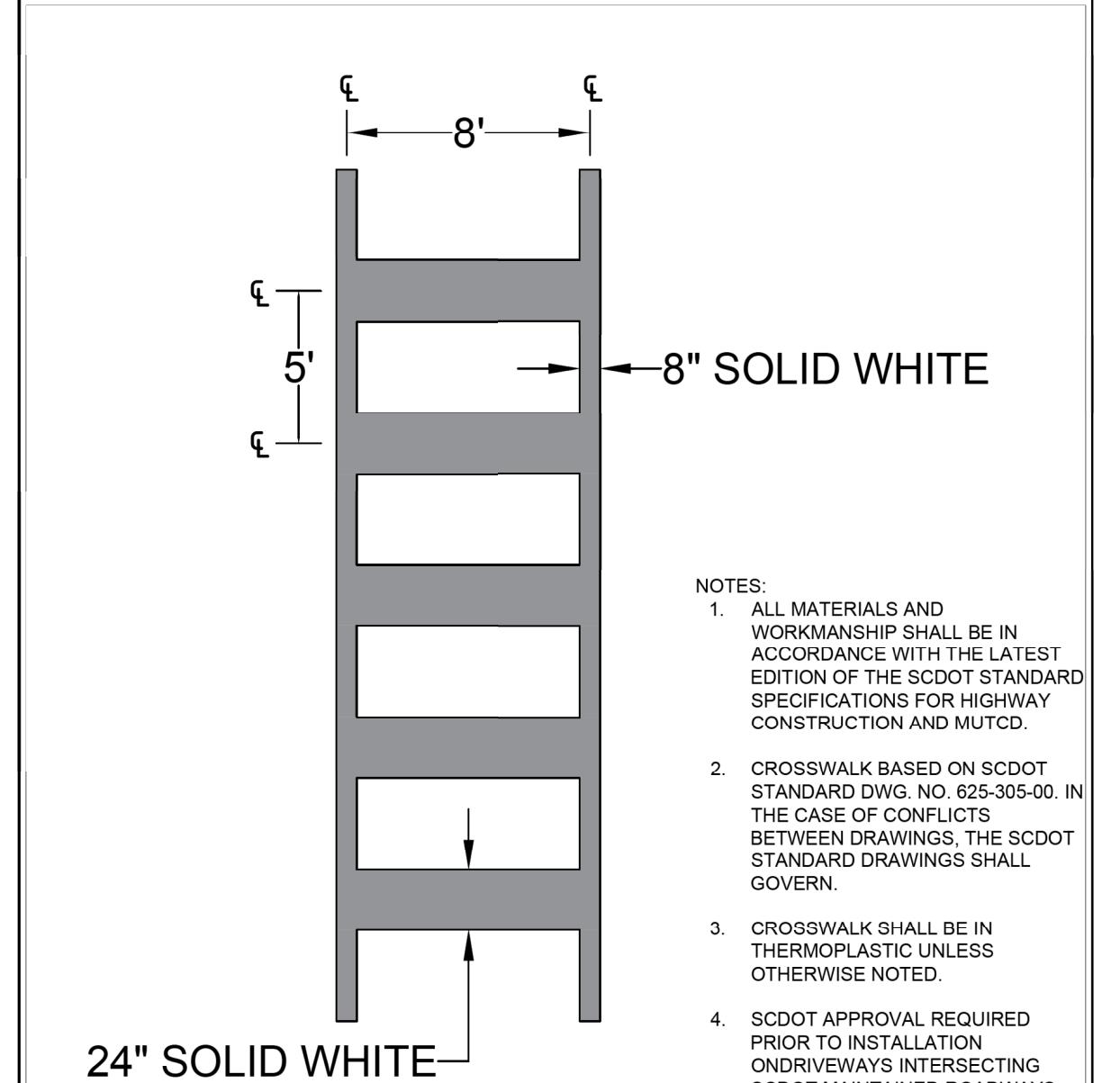
FED. RD. DIV. NO.	STATE	COUNTY	ROADROUTE NO.	SHHEET NO.
3	S.C.	BERKELEY	L-1520	TC1

1. Traffic signs should be designed and placed in accordance with the latest revision of the Manual on Uniform Traffic Control Devices for Streets and Highways (MUTCD), published by the U.S. Department of Transportation
2. Non-traffic related signs within the right-of-way should be approved by the Department of Traffic and Transportation.
3. Regulatory, warning, and guide signs shall be fabricated with ASTM International, formerly American Society for Testing and Materials, Type IV (High-Intensity Prismatic or Microprismatic) or greater retro-reflective sheeting.
4. Street name signs shall be fabricated using extruded aluminum blades 9" in height. The minimum length is 24" with 48" as the maximum. The actual length of the blade will be dictated by the number of letters in the name.
5. The street name color scheme should be white ASTM Type III lettering on a green background.
6. The street name letter font (or typeface) should be FHWA Series "C"; commonly known as Highway Gothic "C".
7. All street names are to consist of 6" upper-case letters with 4.5" lower-case letters. The street designations, e.g., St, Rd, Dr, Cir, Pkwy, Ave, Blvd are to be 50% superscript.
8. The street name, including the superscript designation, shall be centered vertically and horizontally on the blade.
9. The spacing from the blade edge to the beginning and end of the street name, including the superscript designation, should be a minimum of 2".
10. Street name signs should be mounted with a vertical clearance of at least 8' measured from the bottom of the sign to the near edge of the pavement. All other signs shall be mounted at least 7' from the bottom of the sign to the near edge of the pavement, except when a supplemental plaque or secondary sign is permitted.
11. If a supplemental plaque or secondary sign is permitted to be mounted on the same assembly as another sign, the major (most important) sign shall be installed above the supplemental plaque or secondary sign. The minimum height from the bottom of the supplemental plaque or secondary sign to the near edge of the pavement shall be 6'.
12. The minimum lateral offset for installed signs should be 2' measured from the nearest sign edge to the pavement edge (or the face of curb). A minimum offset of 1' from the face of the curb may be used in areas where sidewalk width is limited or where existing poles are close to the curb.
13. All traffic sign posts should be securely installed at least 2' in the ground. Traffic sign posts should be green, 3 lbs/ft steel U-channel posts, breakaway, and 12' in length except for street name sign posts.
14. All street name posts should be round, 2 3/8" OD, breakaway, aluminum or steel, and at least 10' in length.
15. Non-standard post/signs and all attached signs shall be maintained by HOA/POA.

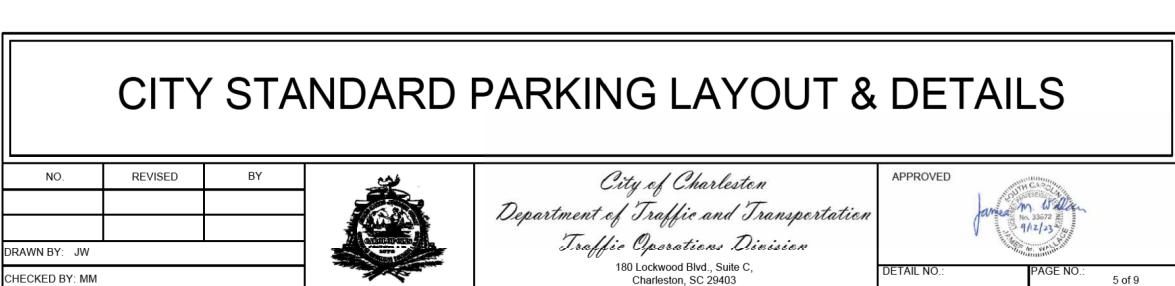
CITY SIGN STANDARDS

NOTES:

1. ALL MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE SCDOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION AND MUTCD.
2. CROSSWALK BASED ON SCDOT STANDARD DWG. NO. 625-305-01. IN THE CASE OF CONFLICTS BETWEEN DRAWINGS, THE SCDOT STANDARD DRAWINGS SHALL GOVERN.
3. CROSSWALK SHALL BE IN THERMOPLASTIC UNLESS OTHERWISE NOTED.
4. SCDOT APPROVAL REQUIRED PRIOR TO INSTALLATION ON SCDOT MAINTAINED ROADWAYS.



CITY STANDARD CROSSWALK - DRIVEWAYS



NOTES:

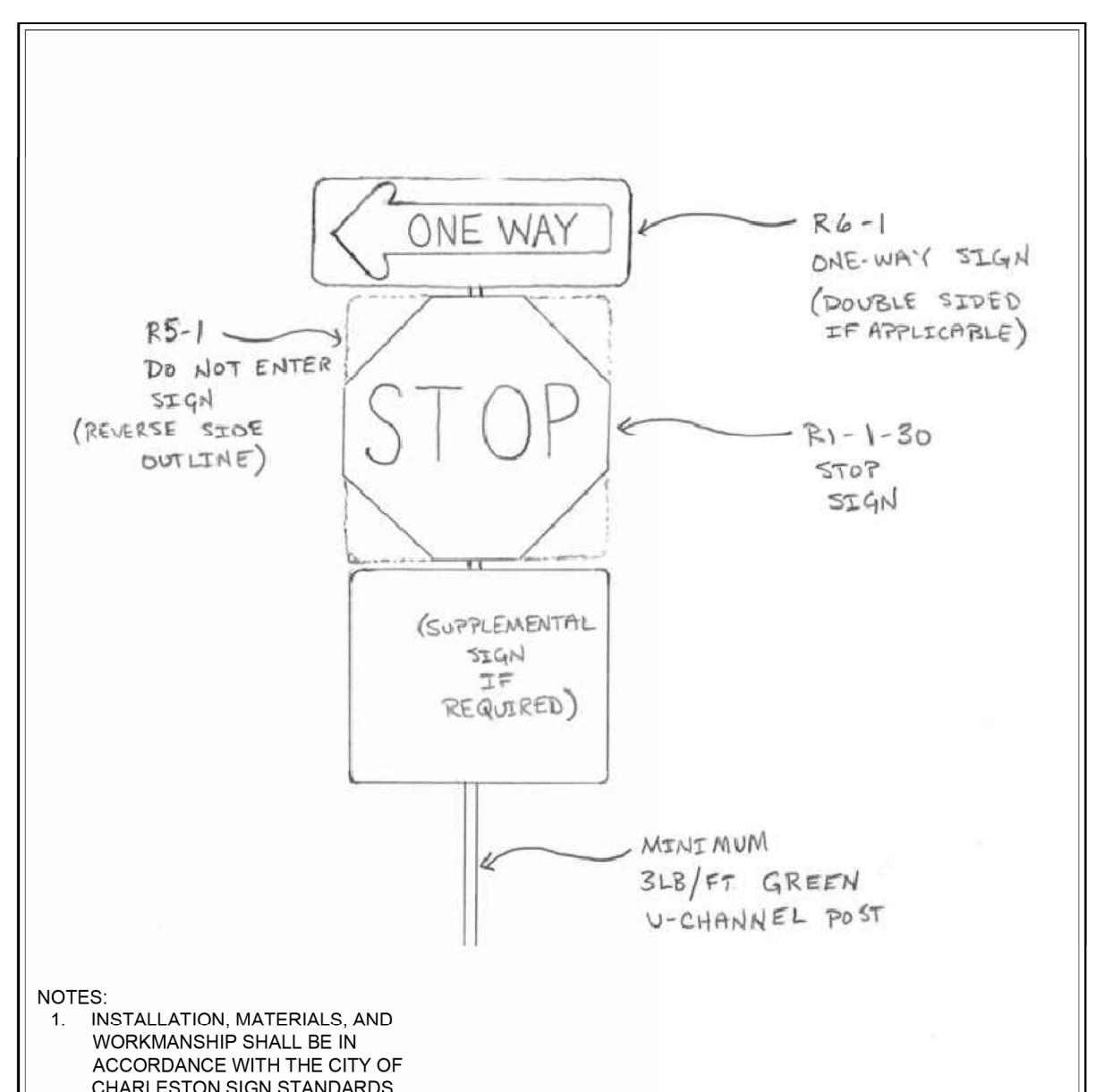
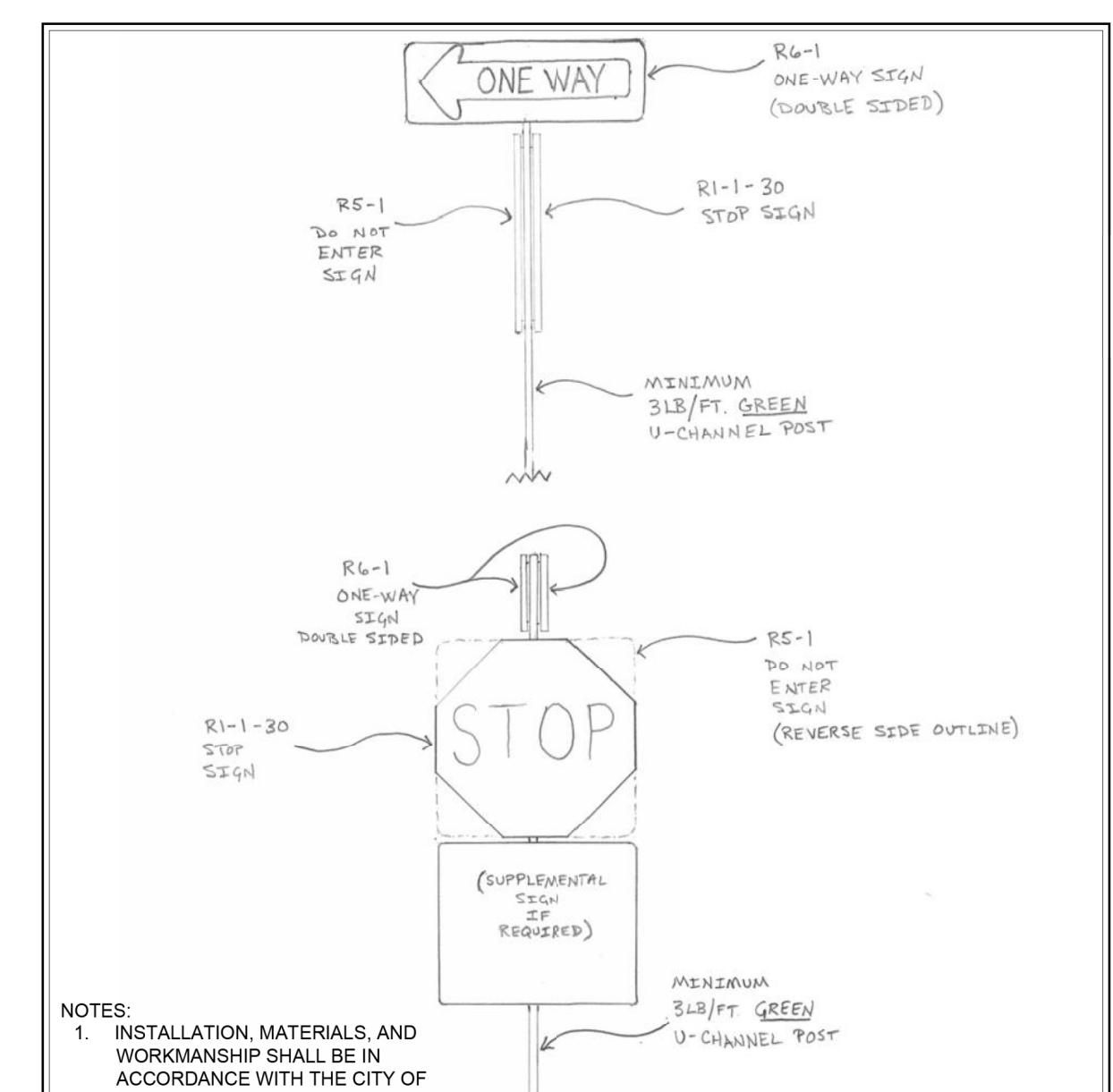
1. ALL MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE SCDOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION AND MUTCD.
2. ARROWS SHALL BE INSTALLED IN THERMOPLASTIC UNLESS OTHERWISE NOTED.
3. SCDOT APPROVAL REQUIRED PRIOR TO INSTALLATION ON SCDOT MAINTAINED ROADWAYS.

Diagram illustrating the dimensions and geometry of a road curve. The plan view shows a curve with a radius of 20.5' and a width of 50'. The longitudinal section shows a transition angle of 55°, an inside curve radius of 20.5', an outside curve radius of 33', and side slopes of 12° and 23.5°. The vertical cross-section shows a height of 24' and a side slope of 9°.

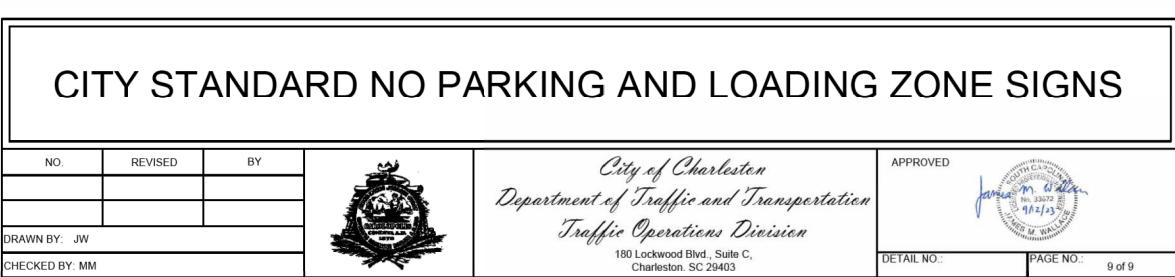
Dimensions and angles:

- TOTAL CURVE LENGTH: 60"
- TOTAL CURVE WIDTH: 50"
- INSIDE CURVE RADIUS: 20.5"
- OUTSIDE CURVE RADIUS: 33"
- INSIDE CURVE SIDE SLOPE: 12°
- OUTSIDE CURVE SIDE SLOPE: 23.5°
- INSIDE CURVE END SLOPE: 29°
- OUTSIDE CURVE END SLOPE: 34°
- TRANSITION ANGLE: 55°
- CROSS-SECTION ANGLE: 9°
- CROSS-SECTION HEIGHT: 24"

CITY STANDARD PAVEMENT ARROWS



CITY STANDARD STOP SIGN WITH MULTIPLE SIGN ARRANGEMENT (CONTINUED)



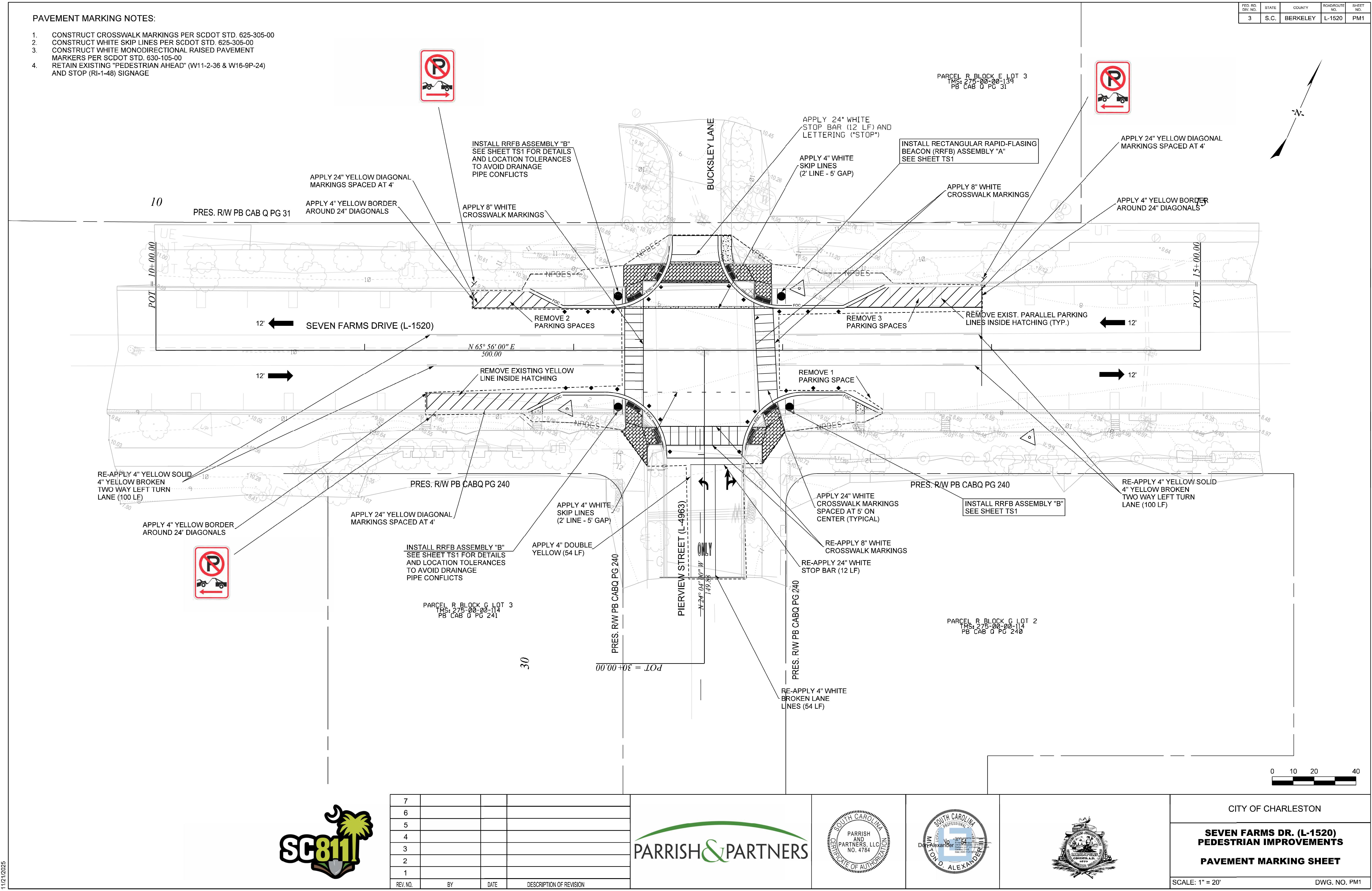
The logo for SC811 is displayed on the right side of the slide. It consists of the letters "SC" in a black, bold, sans-serif font, followed by "811" in a larger, yellow, bold, sans-serif font. To the right of the "11" is a stylized green palm tree with three fronds. Below the letters is a graphic of a shovel with a green handle and a silver blade, resting on a patch of brown ground with green grass at the bottom.

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PARRISH & PARTNERS



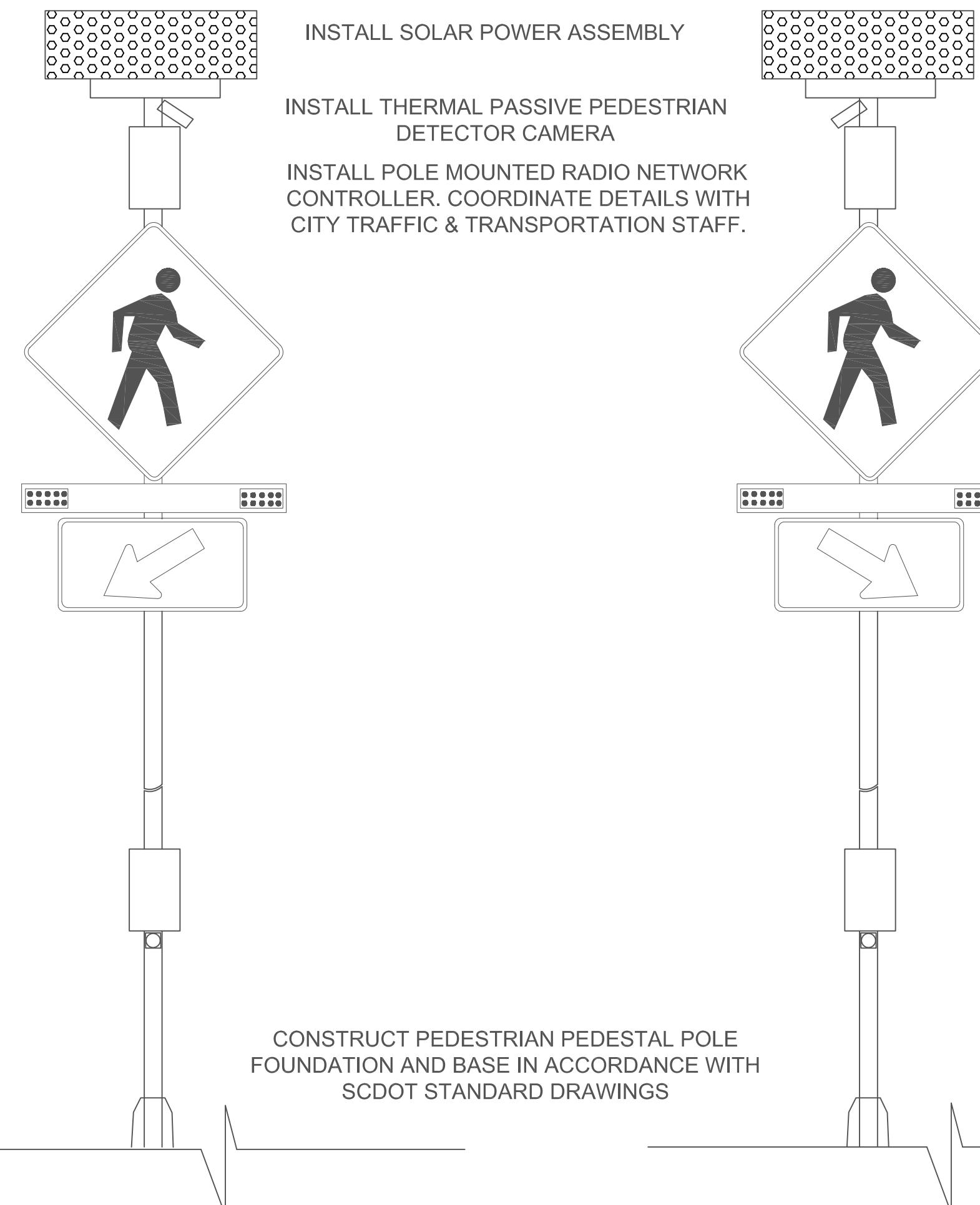
CITY OF CHARLESTON



COUNTY	ROUTE NO.	STREET NAME	SHEET NO.	TOTAL SHEETS
BERKELEY		SEVEN FARMS RD.	TS1	1

NOTES:

1. EACH RRFB SHALL CONSIST OF TWO RAPIDLY-FLASHED RECTANGULAR-SHAPED YELLOW INDICATIONS, EACH WITH AN LED-ARRAY BASED PULSING LIGHT SOURCE. THE SIZE OF EACH RRFB INDICATION SHALL BE AT LEAST 5 INCHES WIDE BY AT LEAST 2 INCHES HIGH.
2. THE TWO RRFB INDICATIONS FOR EACH RRFB UNIT SHALL BE ALIGNED HORIZONTALLY, WITH THE LONGER DIMENSION HORIZONTAL AND WITH A MINIMUM SPACE BETWEEN THE TWO INDICATIONS OF AT LEAST 7 INCHES, MEASURED FROM NEAREST EDGE OF ONE INDICATION TO THE NEAREST EDGE OF THE OTHER INDICATION. THE OUTSIDE EDGES OF THE RRFB INDICATIONS, INCLUDING ANY HOUSINGS, SHALL NOT PROJECT BEYOND THE OUTSIDE EDGES OF THE W11-2 SIGN THAT IT SUPPLEMENTS.
3. WHEN ACTIVATED, THE TWO YELLOW INDICATIONS IN EACH RRFB UNIT SHALL FLASH IN A RAPIDLY FLASHING SEQUENCE.
4. AS A SPECIFIC EXCEPTION TO THE REQUIREMENTS FOR THE FLASH RATE OF BEACONS PROVIDED IN PARAGRAPH 3 OF SECTION 4S.01 (MUTCD 11TH EDITION), RRFBs SHALL USE A MUCH FASTER FLASH RATE AND PROVIDE 75 FLASHING SEQUENCES PER MINUTE.
5. EXCEPT AS NOTED IN NOTE 6 BELOW, DURING EACH 800-MILLISECOND FLASHING SEQUENCE, THE LEFT AND RIGHT RRFB INDICATIONS SHALL OPERATE USING THE FOLLOWING SEQUENCE:
 - THE RRFB INDICATION ON THE LEFT-HAND SIDE SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
 - BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
 - THE RRFB INDICATION ON THE RIGHT-HAND SIDE SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
 - BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
 - THE RRFB INDICATION ON THE LEFT-HAND SIDE SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
 - BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
 - THE RRFB INDICATION ON THE RIGHT-HAND SIDE SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
 - BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
 - BOTH RRFB INDICATIONS SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
 - BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
 - BOTH RRFB INDICATIONS SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
 - BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 250 MILLISECONDS.
6. THE FLASH RATE OF EACH INDIVIDUAL RRFB INDICATION, AS APPLIED OVER THE FULL FLASHING SEQUENCE, SHALL NOT BE MORE THAN 5 FLASHES PER SECOND, TO AVOID FREQUENCIES THAT MIGHT CAUSE SEIZURES.
7. THE LIGHT INTENSITY OF THE YELLOW INDICATIONS DURING DAYTIME CONDITIONS SHALL MEET THE MINIMUM SPECIFICATIONS FOR CLASS 1 YELLOW PEAK LUMINOUS INTENSITY IN THE PUBLICATION "DIRECTIONAL FLASHING OPTICAL WARNING DEVICES FOR AUTHORIZED EMERGENCY, MAINTENANCE, AND SERVICE VEHICLES J595," 2005, SOCIETY OF AUTOMOTIVE ENGINEERS (SAE).
8. THE RRFB SHALL BE NORMALLY DARK, SHALL INITIATE OPERATION ONLY UPON PEDESTRIAN ACTUATION AND SHALL CEASE OPERATION AT A PREDETERMINED TIME AFTER THE PEDESTRIAN ACTUATION. THE DURATION OF THE FLASHING BEACON SHALL BE 19 SECONDS.
9. ALL RRFB UNITS ASSOCIATED WITH A GIVEN CROSSWALK SHALL, WHEN ACTIVATED, SIMULTANEOUSLY COMMENCE OPERATION OF THEIR RAPID FLASHING INDICATIONS AND SHALL CEASE OPERATION SIMULTANEOUSLY.
10. A SMALL LIGHT DIRECTED AT AND VISIBLE TO PEDESTRIANS IN THE CROSSWALK SHALL BE INSTALLED INTEGRAL TO THE RRFB OR PEDESTRIAN PUSH BUTTON DETECTOR TO GIVE CONFIRMATION THAT THE RRFB IS IN OPERATION.
11. PROVIDE AN AUDIBLE PUSHBUTTON INFORMATION MESSAGE:
 - PROVIDE A LOCATOR TONE FOR BUTTON
 - THE AUDIBLE INFORMATION DEVICE SHALL NOT USE VIBROTACTILE INDICATIONS OR PERCUSSIVE INDICATIONS.
 - AUDIBLE MESSAGE SHOULD BE A SPEECH MESSAGE THAT SAYS "WARNING LIGHTS ARE FLASHING". THE AUDIBLE MESSAGE SHOULD BE SPOKEN TWICE.
12. ALL MATERIALS AND WORKMANSHIP TO COMPLY WITH THE MUTCD 11TH EDITION AND SCOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, LATEST EDITION.
13. RRFB POLE LOCATIONS MAY BE ADJUSTED IN THE FIELD WITH CITY APPROVAL TO AVOID DRAINAGE STRUCTURES; HOWEVER CONTRACTOR SHOULD ENSURE THAT RRFB VISIBILITY AND PUSHBUTTON LOCATION AND ACCESSIBILITY MEET MUTCD STANDARDS.



RRFB MODULE WITH SIDE INDICATION

W16-7PL
(24"X12")

**R-10-25
(9"X12")
WITH AUDIBLE
PUSHBUTTON**

FLASHER ASSEMBLY "A"

DETAILS

FLASHER ASSEMBLY "B"

DETAILS

CONSTRUCT AND PROGRAM RRFB ASSEMBLIES PER LATEST SCUDOT AND MUTCD RRFB STANDARDS

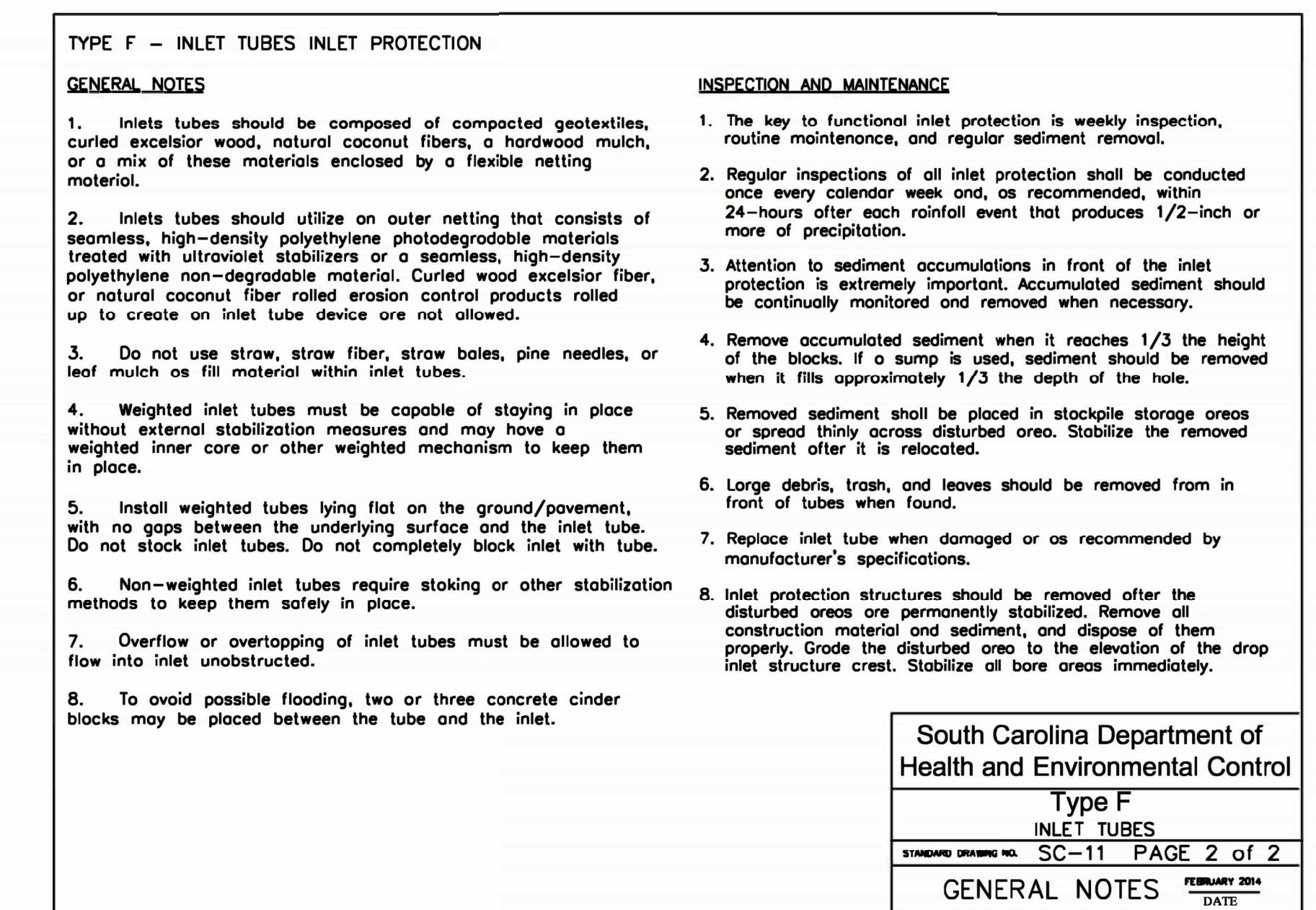
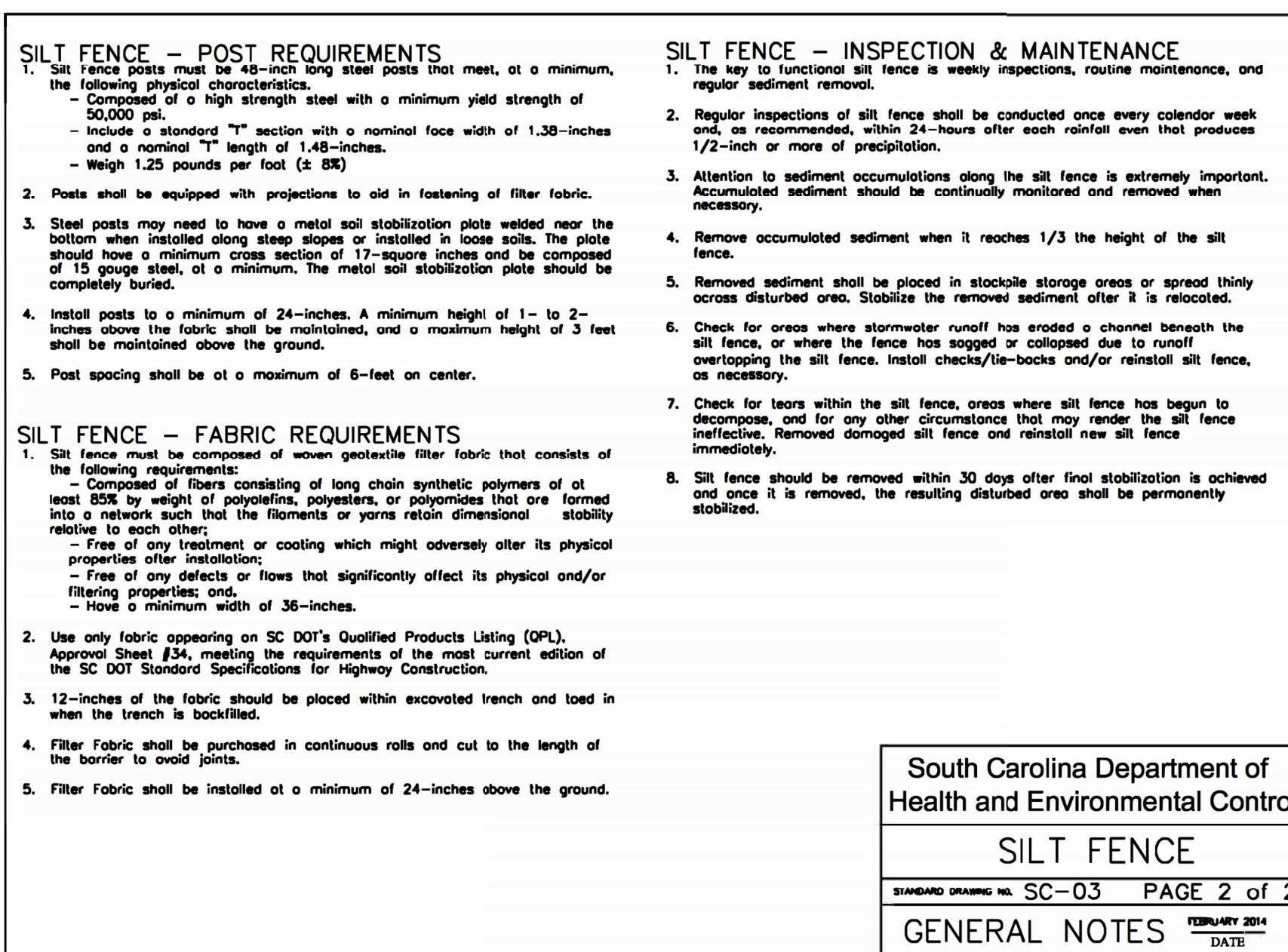
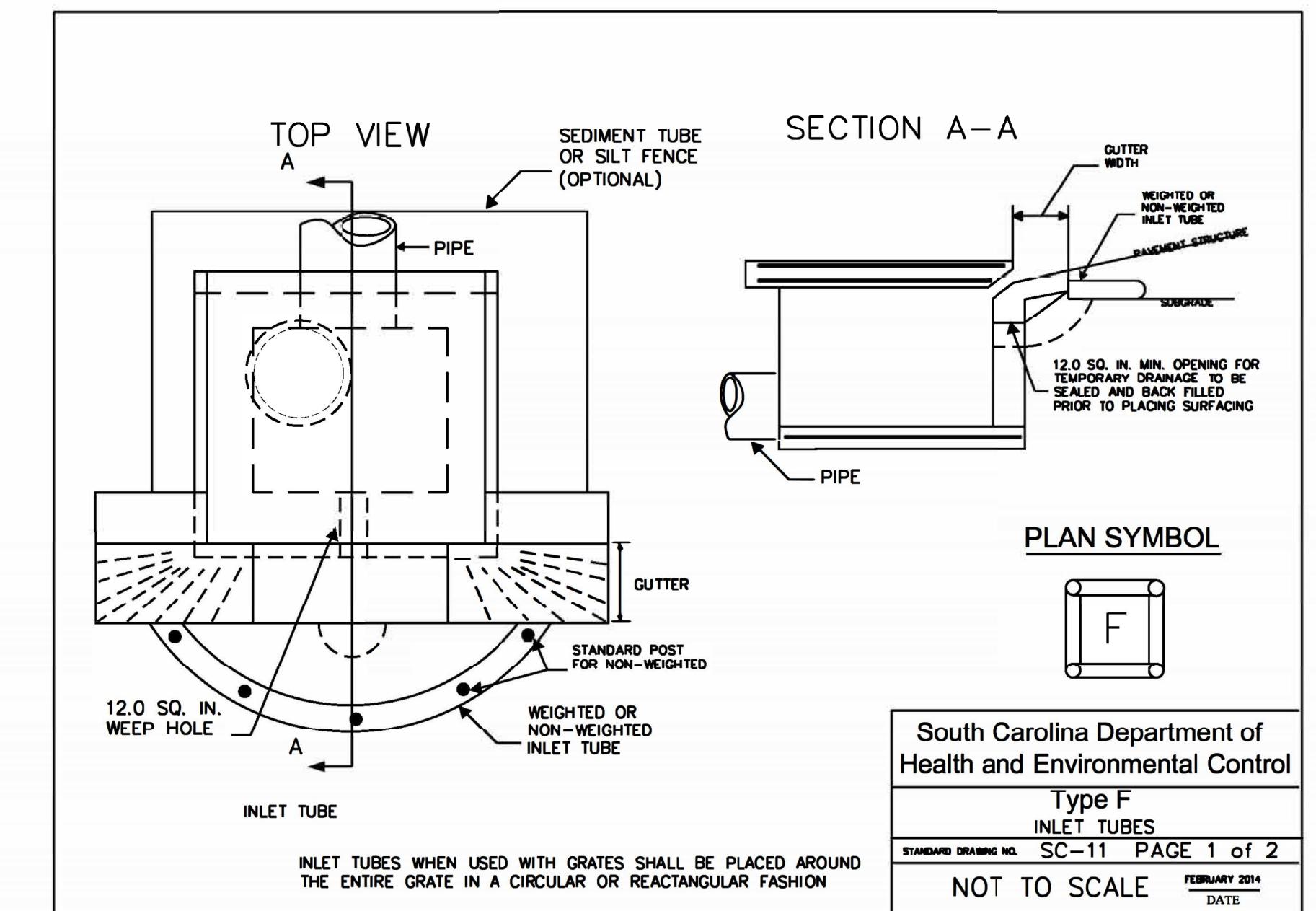
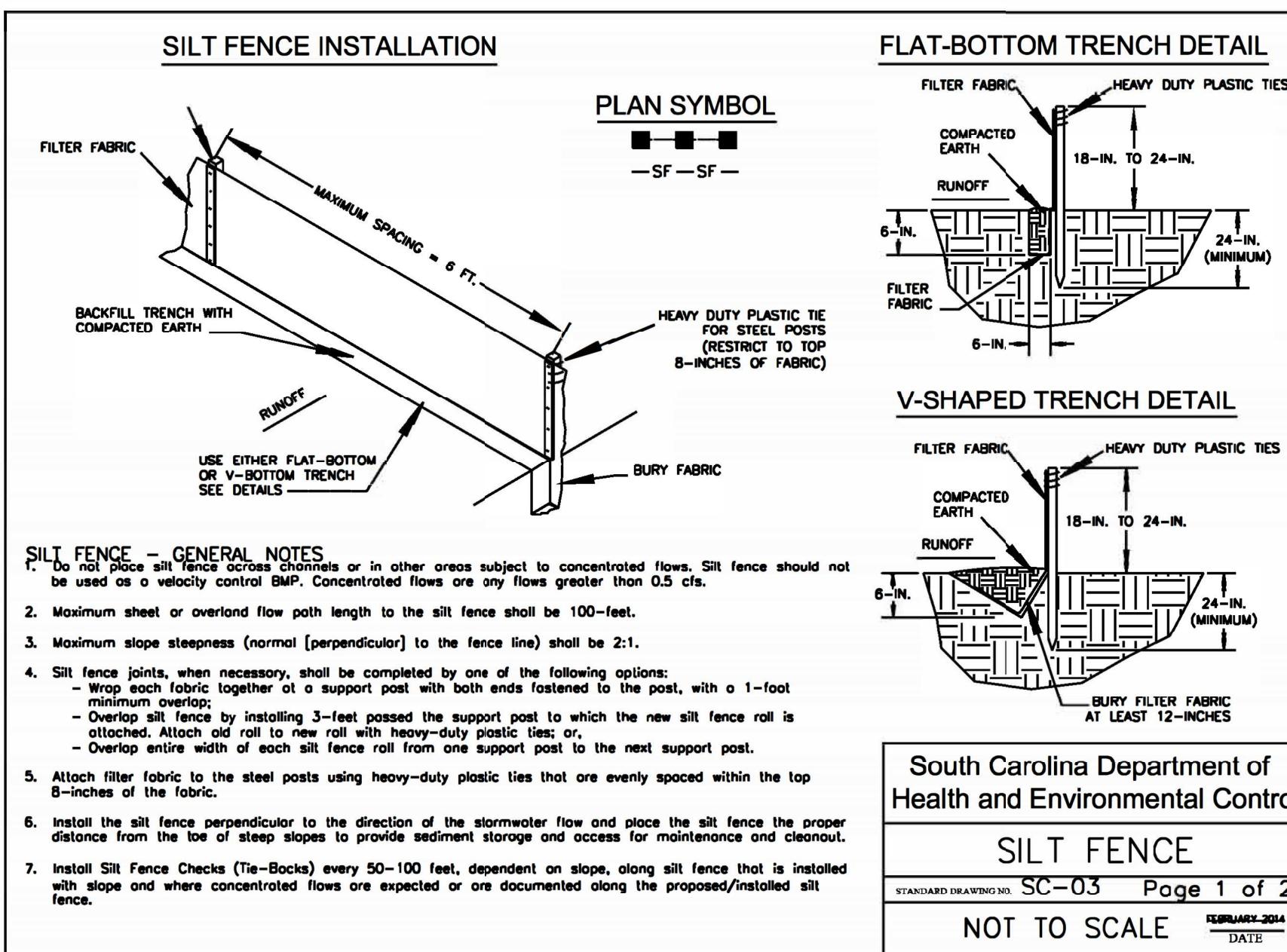


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R/W	_____	DATE _____	SCALE 1" = <u> </u> VERT



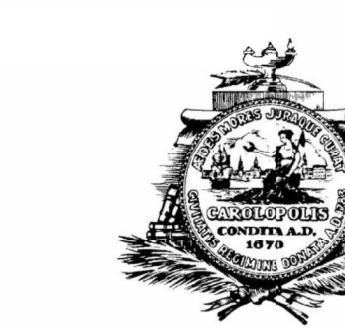
CITY OF CHARLESTON

SEVEN FARMS ROAD AT PIER VIEW
STREET/BUCKSLEY LANE
RECTANGULAR RAPID FLASHING
BEACON (RRFB) SPECIFICATIONS



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PARRISH & PARTNERS



CITY OF CHARLESTON
SEVEN FARMS DR. (L-1520) PEDESTRIAN IMPROVEMENTS
EROSION CONTROL DETAILS

SCALE: NTS

DWG. NO. EC1

EROSION CONTROL NOTES

SCDHEC STANDARD NOTES:

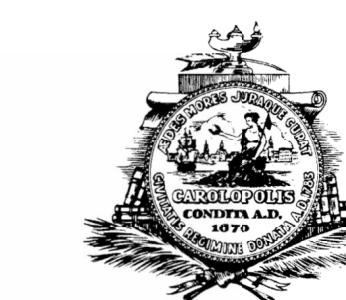
1. IF NECESSARY, SLOPES, WHICH EXCEED EIGHT (8) VERTICAL FEET SHOULD BE STABILIZED WITH SYNTHETIC OR VEGETATIVE MATS, IN ADDITION TO HYDROSEEDING. IT MAY BE NECESSARY TO INSTALL TEMPORARY SLOPE DRAINS DURING CONSTRUCTION. TEMPORARY BERMS MAY BE NEEDED UNTIL THE SLOPE IS BROUGHT TO GRADE.
2. STABILIZATION MEASURES MUST BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN FOURTEEN (14) DAYS AFTER WORK HAS CEASED, EXCEPT AS STATED BELOW.
 - WHERE STABILIZATION BY THE 14TH DAY IS PRECLUDED BY SNOW COVER OR FROZEN GROUND CONDITIONS STABILIZATION MEASURES MUST BE INITIATED AS SOON AS PRACTICABLE.
 - WHERE CONSTRUCTION ACTIVITY ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH DISTURBING ACTIVITIES WILL BE RESUMED WITHIN 14 DAYS, TEMPORARY STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE.
3. AFTER CONSTRUCTION ACTIVITIES BEGIN, INSPECTIONS MUST BE CONDUCTED AT A MINIMUM OF AT LEAST ONCE EVERY CALENDAR WEEK, WITH NO TIME PERIOD BETWEEN INSPECTIONS EXCEEDING 9 DAYS, AND MUST BE CONDUCTED UNTIL FINAL STABILIZATION IS REACHED ON ALL AREAS OF THE CONSTRUCTION SITE. IT IS RECOMMENDED THAT INSPECTIONS ALSO BE CONDUCTED AND BMPS ASSESSED WITHIN 24 HOURS AFTER A STORM EVENT OF 1.0 INCHES OR GREATER AS WELL AS AFTER THE FIRST RAIN EVENT AFTER INITIATION OF CONSTRUCTION ACTIVITIES AND AFTER INSTALLATION OF THE BMPS. IF PERIODIC INSPECTION OR OTHER INFORMATION INDICATES THAT A BMP HAS BEEN INAPPROPRIATELY OR INCORRECTLY INSTALLED, THE PERMITTEE MUST ADDRESS THE NECESSARY REPLACEMENT OR MODIFICATION REQUIRED TO CORRECT THE BMP WITHIN 48 HOURS OF IDENTIFICATION.
4. PROVIDE SILT FENCE AND/OR OTHER CONTROL DEVICES, AS MAY BE REQUIRED, TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEANED, GRADED, AND STABILIZED WITH GRASSING AS SOON AS PRACTICABLE AFTER UTILITY INSTALLATION. FILL, COVER, AND TEMPORARY SEEDING AT THE END OF EACH DAY ARE RECOMMENDED. IF WATER IS ENCOUNTERED WHILE TRENCHING, THE WATER SHOULD BE FILTERED TO REMOVE SEDIMENT BEFORE BEING PUMPED BACK INTO ANY WATERS OF THE STATE.
5. ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFFSITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
6. THE CONTRACTOR MUST TAKE NECESSARY ACTION TO MINIMIZE THE TRACKING OF MUD ONTO THE PAVED ROADWAY FROM CONSTRUCTION AREAS AND THE GENERATION OF DUST. THE CONTRACTOR SHALL DAILY REMOVE MUD/SOIL FROM PAVEMENT, AS MAY BE REQUIRED.
7. RESIDENTIAL SUBDIVISIONS REQUIRE EROSION CONTROL FEATURES FOR INFRASTRUCTURE AS WELL AS FOR INDIVIDUAL LOT CONSTRUCTION. INDIVIDUAL PROPERTY OWNERS SHALL FOLLOW THESE PLANS DURING CONSTRUCTION OR OBTAIN APPROVAL OF AN INDIVIDUAL PLAN IN ACCORDANCE WITH S.C. REG. 72-300 ET SEQ. AND SCR100000.
8. TEMPORARY DIVERSION BERMS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UPSLOPE RUNOFF AND/OR TO DIVERT SEDIMENT-LADEN WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
9. ALL WATERS OF THE STATE (WOS), INCLUDING WETLANDS, ARE TO BE FLAGGED OR OTHERWISE CLEARLY MARKED IN THE FIELD. A DOUBLE ROW OF SILT FENCE IS TO BE INSTALLED IN ALL AREAS WHERE A 50-FOOT BUFFER CAN'T BE MAINTAINED BETWEEN THE DISTURBED AREA AND ALL WOS. A 10-FOOT BUFFER SHOULD BE MAINTAINED BETWEEN THE LAST ROW OF SILT FENCE AND ALL WOS.
10. LITTER, CONSTRUCTION DEBRIS, OILS, FUELS, AND BUILDING PRODUCTS WITH SIGNIFICANT POTENTIAL FOR IMPACT (SUCH AS STOCKPILES OF FRESHLY TREATED LUMBER) AND CONSTRUCTION CHEMICALS THAT COULD BE EXPOSED TO STORMWATER MUST BE PREVENTED FROM BECOMING A POLLUTANT SOURCE IN STORMWATER DISCHARGES.
11. A COPY OF THE SWPPP, INSPECTIONS RECORDS, AND RAINFALL DATA MUST BE RETAINED AT THE CONSTRUCTION SITE OR A NEARBY LOCATION EASILY ACCESSIBLE DURING NORMAL BUSINESS HOURS, FROM THE DATE OF COMMENCEMENT OF CONSTRUCTION ACTIVITIES TO THE DATE THAT FINAL STABILIZATION IS REACHED.
12. INITIATE STABILIZATION MEASURES ON ANY EXPOSED STEEP SLOPE (3H:1V OR GREATER) WHERE LAND-DISTURBING ACTIVITIES HAVE PERMANENTLY OR TEMPORARILY CEASED, AND WILL NOT RESUME FOR A PERIOD OF 7 CALENDAR DAYS.
13. MINIMIZE SOIL COMPACTION AND, UNLESS INFEASIBLE, PRESERVE TOPSOIL.
14. MINIMIZE THE DISCHARGE OF POLLUTANTS FROM EQUIPMENT AND VEHICLE WASHING, WHEEL WASH WATER, AND OTHER WASH WATERS. WASH WATERS MUST BE TREATED IN A SEDIMENT BASIN OR ALTERNATIVE CONTROL THAT PROVIDES EQUIVALENT OR BETTER TREATMENT PRIOR TO DISCHARGE.
15. MINIMIZE THE DISCHARGE OF POLLUTANTS FROM DEWATERING OF TRENCHES AND EXCAVATED AREAS. THESE DISCHARGES ARE TO BE ROUTED THROUGH APPROPRIATE BMPS (SEDIMENT BASIN, FILTER BAG, ETC.)

pw:\parish-pw\bentley.com:parrish-pw\Documents\Surface Transportation\City of Charleston\Seven Farms at Pier View\Roadway\Sheets\SevenFarms_SHEET-EC1-EROSION CONTROL NOTES.dgn

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2			
1			
REV. NO.	BY	DATE	DESCRIPTION OF REVISION



PARRISH & PARTNERS



CITY OF CHARLESTON

**SEVEN FARMS DR. (L-1520)
PEDESTRIAN IMPROVEMENTS**

EROSION CONTROL NOTES

CALE: NTS

DWG. NO.

SEQUENCE OF CONSTRUCTION

1. PRE CONSTRUCTION MEETING
2. NOTIFY SCDES EQC REGIONAL OFFICE 48 HOURS PRIOR TO BEGINNING LAND DISTURBING ACTIVITIES.
3. INSTALL CONSTRUCTION ENTRANCES AND EROSION CONTROL MEASURES (SILT FENCE, ETC).
4. CLEARING & GRUBBING OF SITE.
5. INSTALL NEW CURB & GUTTERS AND STORM DRAINAGE SYSTEM. PLACE INLET PROTECTION AS EACH INLET IS INSTALLED.
6. PERFORM FINE GRADING, INSTALL PAVING, SIDEWALKS, ETC.
7. INSTALL PERMANENT/FINAL STABILIZATION.
8. REMOVE TEMPORARY EROSION CONTROL MEASURES AFTER DRAINAGE STRUCTURE AREA IS FINALLY STABILIZED.
9. APPLY PERMANENT PAVEMENT MARKING AND SIGNING

NOTE: MAINTENANCE OF SEDIMENT AND EROSION CONTROL MEASURES MUST CONTINUE UNTIL THE SITE IS PERMANENTLY STABILIZED AND THE MEASURES ARE REMOVED.

